



## **TRANSPORTATION COMMISSION MEETING**

City Council Chambers, 33 East Broadway Avenue Meridian, Idaho  
Monday, November 01, 2021 at 3:30 PM

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### **Agenda**

#### **VIRTUAL MEETING INSTRUCTIONS**

To join the meeting online: <https://us02web.zoom.us/j/81023239736>

Or join by phone: 1-669-900-6833

Webinar ID: 810 2323 9736

#### **ROLL-CALL ATTENDANCE**

<input type="checkbox"/> David Ballard	<input type="checkbox"/> Luke Cavener, Ex-Officio - City Council
<input type="checkbox"/> Zachary Shoemaker	<input type="checkbox"/> Shawn Martin, Ex-Officio - ACHD
<input type="checkbox"/> Ryan Lancaster	<input type="checkbox"/> Toni Tisdale, Ex-Officio - COMPASS
<input type="checkbox"/> Walter Steed	<input type="checkbox"/> RD Huntley, Ex-Officio - WASD
<input type="checkbox"/> Tracy Hopkins	<input type="checkbox"/> Alissa Taysom, Ex-Officio - VRT
<input type="checkbox"/> Stephen Lewis	<input type="checkbox"/> Justin Price, Ex-Officio - ITD
<input type="checkbox"/> Jared Smith	
<input type="checkbox"/> Tom LeClaire	
<input type="checkbox"/> Joseph Leckie	

#### **ADOPTION OF AGENDA [ACTION ITEM]**

#### **APPROVAL OF MINUTES [ACTION ITEM]**

1. October 4, 2021

#### **OLD BUSINESS [ACTION ITEM]**

2. Transportation Projects Update - M. Carson

#### **NEW BUSINESS [ACTION ITEM]**

3. Valley Regional Transit Service Update - Stephen Hunt
4. Introduction of New Traffic Sergeant - W. Steed

5. Meridian Police Department Update - Sergeant Justin Dance
- [6.](#) Chinden Access West of Hwy-16 - M. Carson
- [7.](#) Staff Communications - M. Carson
- [8.](#) ACHD Capital Investment Citizens Advisory Committee Volunteer - W. Steed
9. January Meeting Date - W. Steed

**FUTURE MEETING TOPICS**

10. Field Trip 2022
11. Fields Subarea Plan

**NEXT MEETING - December 6, 2021**

**ADJOURNMENT [ACTION ITEM]**

# Meeting Minutes

## Meridian Transportation Commission

### October 4, 2021

#### Virtual Meeting Information

<https://us02web.zoom.us/j/82913177418>

Or join by phone: Dial 669-900-6833 and enter Webinar ID: 82913177418

**(Meeting called to order at approximately 3:30 p.m.; Zoom Meeting started and roll taken)**

<u>  x  </u> Walter Steed	<u>  x  </u> Stephen Lewis	<u>  x  </u> Shawn Martin (Ex-Officio-ACHD) (via zoom)
<u>  x  </u> David Ballard	<u>  x  </u> Jared Smith	<u>  x  </u> Toni Tisdale (Ex-Officio-COMPASS)
<u>  x  </u> Ryan Lancaster	<u>  o  </u> Zachary Shoemaker (via-zoom)	<u>  x  </u> RD Huntley (Ex-Officio-WASD)
<u>  o  </u> Tom LeClaire (via-zoom)	<u>  x  </u> Joseph Leckie	<u>  o  </u> Alissa Taysom (Ex-Officio VRT)
<u>  x  </u> Tracy Hopkins	<u>  o  </u> Luke Cavener, (Ex-Officio via Zoom)	<u>  x  </u> Mark Wasdahl for Justin Price (Ex-Officio ITD) (via zoom)

Others Present: Edinson Bautista, T. Baird, M. Carson, Sgt. B. Frasier

### Adoption of the Agenda

Recording was not activated so motion and approval were not available

#### I. Approval of August 2<sup>nd</sup>, 2021 meeting minutes

Motion and approval were noted on hardcopy of Agenda provided no changes were noted. A motion was made by Commissioner Ballard to approve August 2<sup>nd</sup> meeting minutes; second by Commissioner Hopkins – all ayes- motion passed.

Chairman Steed acknowledge Mr. Huntley as the new WASD Ex-Officio to the Transportation Commission. Mr. Huntley gave a brief introduction of himself.

### Old Business Items

#### 2. Transportation Projects Update (M. Carson)

As is the normal practice, Ms. Carson provided the Transportation Projects Update as part of the meeting packet. Ms. Carson noted new information in bold and highlighted noteworthy information orally during the meeting.

The following is a summary of updates Ms. Carson provided the Commission:

The ACHD Commission will review the revised 2022-2026 Initial Draft of the Integrated Five Year Work Plan (IFYWP) at their work session on October 6th.

The design phase of the Linder overpass and widening project is being initiated. A project scope for a design RFQ and an interagency agreement between ACHD, ITD and the City are being drafted.

Eagle, Amity to Victory; Ustick/Black Cat; and 8<sup>th</sup> St Crossing projects have reached substantial completion.

Recessed multiuse pathway crossings will not be included in the Locust Grove, Victory to Overland improvements.

ACHD's upcoming residential maintenance projects include several complete streets projects in downtown Meridian. The full list is supplied in the packet.

Updates on the Hwy-16 project are available in the Project Updates and Staff Communications memos.

## **New Business Items**

### **3. McMillan, Ten Mile to Linder Concern (Girl Scout Troop 401)**

Recording of the audio began at this point in time.

Chairman Steed invited Girl Scout Troop 401 and their adult companions to the podium for introductions and to address the Commission.

Troop 401 consists of 3<sup>rd</sup> and 4<sup>th</sup> graders that meet at River Valley Elementary. They presented their Take Action Project as part of badge requirement. The Troop brainstormed ideas regarding safety in their community and then came up with two proposed solutions. The issue is providing a safe route across McMillan between two neighborhoods that have several girl scout members. The Troop shared a visual presentation of the issue and then discussed the two proposed options:

- I. Creating a Crosswalk and paved sidewalk closer to the Hunter Elementary school at Palatine Way
- II. Building a pedestrian bridge closer to the neighborhoods impacted at Goddard Creek way and McMillan.

The Troop concluded their presentation and Chairman Steed praised their efforts to come before the Commission to present and thanked them for their input.

Chairman Steed opened the floor for discussion which took place. Mr. Hood noted that this project is currently part of the Draft Meridian Priorities and is listed as #7 on the list. The description is McMillan Road, Ten Mile to Linder and includes a crossing at Goddard Creek Way and Wild Goose Way to be designed in 2024. The Draft priorities are not currently adopted by ACHD but this is in the top 10 for the City.

#### **4. Meridian Police Department (Lt. B. Frasier)**

Lt Frasier was not in attendance at the time of this item so no update was provided. This item will be held over for future meetings or when Lt Frasier arrives.

#### **5. Staff Communications (M. Carson)**

Ms. Carson noted that the Staff Communication Memo begins on page 16 of the packet.

Ms. Carson will highlight a few of the items and began by noting the request for a sidewalk at Black Cat at Gondola. Kendal Kemmer at ACHD has confirmed that WASD has a plan to extend and construct a crosswalk to the new Pleasant View Elementary School. ACHD and WASD will work together to get this planned, designed and constructed.

On page 17, Ms. Carson noted that the Meridian City Council and ACHD Commission held their joint meeting on September 20<sup>th</sup>. The full meeting can be viewed at the link following link:  
<https://www.youtube.com/watch?v=EpY6bNftSyQ>

Ms. Carson gave a brief summary of the issues discussed around Multi-Use Pathways for pedestrians and bicyclists, the 2022 legislative coordination of HB389, and ITD Highway 16 update.

Ms. Carson congratulated Joseph Leckie on his reappointment as the Youth Commissioner on the Transportation Commission. Commissioner Leckie's appointment will run through August 31, 2022.

Ms. Carson continued through the Staff Communications and noted that she would not update about SH16 since she did that during the projects update.

Ms. Carson noted a citizen request for transit and highlighted the Deer Crest restricted parking issue that was brought forth to the Commission back in June/July. The citizen requesting the time restricted parking, Mr. Peters was not able to secure the needed signatures to move this forward so it is a closed issue at this time. Ms. Carson advised Mr. Peters on the parking code and enforcement procedures in his area.

On Page 20 Ms. Carson noted the Master Street Map and the request for comments from ACHD. A summary of comments is provided in the packet for your review. Only sections of the master street map that staff provided comments on are included in the packet.

Lastly, Ms. Carson noted the Keeping Up With COMPASS publication included in the packet and then spoke on the COMPASS traffic issue. The City has received a couple citizen concerns about traffic on Black Cat between Pine and Franklin as it relates to the new Compass Charter School for morning arrivals and afternoon departures. Lt. Harper of the MPD Traffic Team has worked with the school to prepare and disseminate messaging to the parents to help quell those issues. Busing shortages have exacerbated the traffic problem, another outcome of the Covid pandemic.

Ms. Carson directed the attention of the Commission to the ACHD message from Kendell Kemmer in the packet and noted ACHD's design plan for a southbound left turn lane on Black Cat. This will occur as part of the expansion of Black Cat project and be addressed in the regular integrated five-year work plan. Ms. Carson suggested that the Prioritization subcommittee review this information and take this item into consideration as they assess the prioritization priorities of the City.

Ms. Carson stood for questions and Chairman Steed opened the floor for discussion on the Staff Communication topics, or any other topic the Commission wished to address, which took place.

Chairman Steed reviewed the Future Meeting Topics and asked for any needed updates. None were noted which completed the Agenda topics.

### **Future Meeting Topics**

- 6. Harvest and VRT Service Update (November)**
- 7. ACHD Bike and Pedestrian Treatment**

**Next Meeting Date: November 1<sup>st</sup>, 2021**

### **Adjourn**

Chairman Steed asked for a motion to adjourn, which was made by Commissioner Hopkins; second by Commissioner Ballard. Meeting adjourned at 4:30.

(AUDIO RECORDING AVAILABLE UPON REQUEST)

APPROVED:

\_\_\_\_\_  
WALTER STEED, CHAIR

\_\_\_\_\_  
DATE

ATTEST:

\_\_\_\_\_  
CHRIS JOHNSON (City Clerk)

\_\_\_\_\_  
DATE



# MEMO TO TRANSPORTATION COMMISSION

## Agenda Topic on the Transportation Commission Agenda

**From:** Miranda Carson, Coordination Planner      **Meeting Date:** November 1, 2021  
**Presenter:** Miranda Carson      **Estimated Time:** 10 minutes  
**Topic:** Transportation Projects Update

Below is a summary/update on some of the transportation and roadway projects City Staff has been involved with recently. This is not an exhaustive list, but rather highlights some of the more important activities that have recently transpired (or are about to occur) in the transportation realm. Staff will be at the meeting to discuss some of these topics in more detail. Please feel free to contact staff should you have any comments, concerns, or questions on any of these projects. New information is in **bold**.

### KEY ACHD PROJECTS:

ACHD Integrated Five-Year Work Program: The 2022-2026 Initial Draft of the Integrated Five-Year Work Plan (IFYWP) is in the process of being revised due to funding availability in the Fiscal Year 2022 Adopted Budget. The public and partner comment period will be open November 1-15<sup>th</sup>. More information can be found here:

[https://www.achdidaho.org/Departments/PlansProjects/IFYWP\\_2226draft.aspx](https://www.achdidaho.org/Departments/PlansProjects/IFYWP_2226draft.aspx).

The 2021-2025 IFYWP was adopted by the ACHD commission on September 23<sup>rd</sup>, 2020 and can be found here: <https://www.achdidaho.org/Departments/PlansProjects/IFYWP.aspx>  
*City Staff Contact: Miranda Carson, Community Development*

Linder, Overland to Franklin: The design phase of this project is being initiated. **The RFQ for design is open. Responses are being accepted until November 3<sup>rd</sup>.** An interagency agreement between ACHD, ITD and the City are being drafted. An interagency Task Force has been established by the Mayor with the purpose of ensuring the Linder Overpass is realized. City Council approved a budget allocation of \$2.5 million dedicated to of building this critical overpass in April 2021. This project was submitted to COMPASS for consideration of a Project Development Grant for \$25,000. Widen Linder Rd to 5 lanes with curb, gutter, sidewalk and a Level 3 bike facility from Overland Rd to Franklin Rd as per the 2020 CIP and the Bike Master Plan. Project includes a new 4-lane I-84 overpass. Joint project between ACHD and ITD. This project is listed as future in the adopted IFYWP.

*City Staff Contact: Miranda Carson, Community Development*

Eagle Road, Amity to Victory and roundabout at Amity: **Minor punch list remaining.** Work on the west side of Eagle Road is ongoing. Landscape on west being placed, and sidewalk is ongoing. Amity/Eagle roundabout open. Zaldia roundabout is complete. Pedestrian crossing at

Ten Mile Creek is functional. ACHD is now including shark teeth and additional signage in the design at Rome. City staff continue to advocate that the landscaping at Rome Dr. will create visibility issues. Widen Eagle to 5 lanes with curb, gutter, multi-use pathways. Includes a 2-lane roundabout with a slip lane at the northwest corner at Amity for southbound traffic turning west. Also includes a pedestrian crossing on Rome Dr. Design will be done with in-house, ACHD resources. NOTE: There is insufficient right-of-way on Amity west of Eagle to allow the striping of the bike lane on the north side of the road, so it will only be the two travel lanes westbound, the center turn lane, and one eastbound travel lane until ACHD can program a project there in the future and widen to the south. The west side of Eagle Road will be fully improved including a multi-use path. Here is the project website: [http://achdidaho.org/Projects/proj\\_road\\_eagle-road-amity-road-to-victory-road.aspx](http://achdidaho.org/Projects/proj_road_eagle-road-amity-road-to-victory-road.aspx)

*City Staff Contact: Brian McClure, Community Development*

McMillan/Black Cat Intersection: Signalize and expand the McMillan Road and Black Cat Road Intersection to a 3x3 configuration. Pedestrian facilities will be added at the intersection in the form of ADA-compliant ramps and signal infrastructure; no additional sidewalk will be constructed beyond what is needed for the pedestrian ramps. The new traffic signal at the intersection will accommodate the 3x3 configuration with flashing yellow arrow capability for the left-turn movements if desired. This project is currently programmed for ROW in FY 2021 and construction in FY 2022.

Locust Grove Road, Victory to Overland: The original 95% design with bike lanes is being revised to include a multi-use path (with no bike lanes) and at-grade side street crossings where the crosswalk is recessed from Locust Grove Rd. **ACHD does not plan to include recessed crossings in this design.** The hawk signal will be moved from just south of Mastiff to north of Mastiff, between Mastiff and Puffin; despite City staff request, there is not a pedestrian refuge island currently planned. Widen Locust Grove to 5 lanes with curb, gutter, multi-use pathway. Additional ROW acquisition due to design changes is now anticipated through FY 2022, and construction is anticipated FY2022. Here is the project website:

[http://achdidaho.org/Projects/proj\\_road\\_locust-grove-victory-to-overland.aspx](http://achdidaho.org/Projects/proj_road_locust-grove-victory-to-overland.aspx)

*City Staff Contact: Brian McClure, Community Development*

Victory/Locust Grove Roundabout: The design of the multi-use path connection with the roundabout is being revised. Redesigned 99% plans should be received in August or early September. The schedule has also changed. ROW will now continue through FY 2022 and Construction will be in FY2023. Construct a multi-lane roundabout. Projects includes replacement/widening of bridge #2116 over the Tenmile Creek on Victory Rd and connection on the north end of a sidewalk gap to the west. This project was done with in-house design. Here is the project website:

[http://achdidaho.org/Projects/proj\\_road\\_locust-grove-victory-to-overland.aspx](http://achdidaho.org/Projects/proj_road_locust-grove-victory-to-overland.aspx)

*City Staff Contact: Brian McClure, Community Development*

Ten Mile Road, Victory to Overland and Ten Mile/Victory Roundabout: **The project is going through a redesign for one-way bike facilities behind the back of curb.** Center medians have now been added to portions of the corridor. The design now also includes a multiuse pathway and side street crossings where the crosswalk is recessed. An updated design can be found on the project webpage. City staff expressed concerns to ACHD staff that this is not the design that was shared with the public during the open comment period. The sidewalk level bike lanes will continue several hundred feet on the south leg of the Ten Mile/Victory intersection before transitioning to a rural cross-section. Notice to proceed has been issued for November



2021. Widen Ten Mile to 5-lanes with curb, gutter, and multi-use pathway. In the IFYWP, this project was designed in 2019, right-of-way in 2020-2021, and construction in 2022-2023 at a cost of \$3.9M. Here is the project website:

[http://achdidaho.org/Projects/proj\\_road\\_ten-mile-victory-to-overland.aspx](http://achdidaho.org/Projects/proj_road_ten-mile-victory-to-overland.aspx)

*City Staff Contact: Brian McClure, Community Development*

Eagle Road, Lake Hazel to Amity: **Design is planned to be complete by summer 2022. A landscape planting plan is being created for the basin adjacent to this project.** Design concept includes center medians; City Council opted not to request the center medians be landscaped on this project. Construction may be delayed to 2024. Widen Eagle to 5 lanes with curb, gutter, sidewalk and a multi-use pathway. Includes a multi-lane roundabout at Eagle Rd and Taconic Dr. ROW is programmed in FY 2022. Construction is planned for 2023 in the adopted IFYWP with a cost of \$3.4M. Here is the project website:

[http://www.achdidaho.org/Projects/proj\\_road\\_eagle-lake-hazel-to-amity.aspx](http://www.achdidaho.org/Projects/proj_road_eagle-lake-hazel-to-amity.aspx)

*City Staff Contact: Miranda Carson, Community Development*

Lake Hazel and Eagle Signal: **ACHD will consider raised bike lane or multiuse pathway to be designed into project. Construction will be delayed to 2024.** 95% design has been distributed to the project. The design includes significant grade changes of the existing intersection and will integrate large retaining walls. City staff continue to have concerns with the recessed pathway at E Levin Dr. Notice to proceed has been issued for January 2023. Widen and signalize intersection to 4/5 lanes on Eagle and 3/4 lanes on Lake Hazel. Design is happening this calendar year, right-of-way in 2021-2022, and construction in 2022 in the IFYWP. Design details are on the project webpage. Here is a link to the project website:

[http://achdidaho.org/Projects/proj\\_intersection\\_lake-hazel-and-eagle.aspx](http://achdidaho.org/Projects/proj_intersection_lake-hazel-and-eagle.aspx)

*City Staff Contact: Brian McClure, Community Development*

Fairview, Locust Grove to Eagle & Locust Grove Signal: Bike lane treatment is an ongoing discussion. ACHD has a consultant that will be providing recommendations for how to handle bike crossings. Bike lane will likely be separated and elevated from the vehicle travel lanes. ACHD is planning to replace existing walks, to address ADA issues, but keep detached where possible in same locations to the degree possible. ACHD is not planning a crossing at Fairview and Stonehenge, with the reason cited being safety concerns. A Z-crossing just west of Fairview/Webb is being considered. Widen intersection to 9 lanes on Fairview and 7 lanes on Locust Grove. Widen Fairview to 7 lanes with curb, gutter, sidewalk and Level 3 bike facility. Design in 2020-2021, right-of-way acquisition in 2021-22, and construction in 2023 in adopted IFYWP with a cost of \$4.26M. Construction may be delayed to 2024. Additional business outreach is planned as well. Here is a link to the project website:

[https://www.achdidaho.org/Projects/proj\\_road\\_fairview-avenue-locust-grove-road-to-eagle-road.aspx](https://www.achdidaho.org/Projects/proj_road_fairview-avenue-locust-grove-road-to-eagle-road.aspx)

*City Staff Contact: Caleb Hood, Community Development*

Ustick, Ten Mile to Linder: **Center medians are planned as part of this project.** Concept work continues with the design team. Likely significant impacts to landscape buffers on south side of the street, and a retaining wall necessary along Five Mile Creek. ACHD tentatively looking at two pedestrian crossings. Design will include multiuse pathways with 4-foot minimum buffer. No sidewalk/pathways along Five Mile creek where retaining wall to be located; connection will rely on City pathway on north side of creek. Widen Ustick Rd to 5 lanes with curb, gutter, and multi-use pathway. from Ten Mile Rd to Linder Rd as per the 2020 CIP and the Bike Master Plan. Concept design will further clarify project scope. Construction is planned for 2025. Here is a link

to the project website: [http://achdidaho.org/Projects/proj\\_road\\_ustick-road-ten-mile-to-linder.aspx](http://achdidaho.org/Projects/proj_road_ustick-road-ten-mile-to-linder.aspx)

*City Staff Contact: Caleb Hood, Community Development*

Amity Rd and Locust Grove Rd Signal: **Design kick off began on the ultimate configuration October 20th. ACHD is considering both a roundabout and a signal. The full scope is not yet determined, but the project will include sidewalks or a pathway to Mary McPherson Elementary.** The interim project was terminated by ACHD. Instead a more final full overhaul of the intersection under a larger project will likely be constructed in FY24 or FY25.

*City Staff Contact: Brian McClure, Community Development*

Lake Hazel Road, Eagle to Cloverdale: **Kick off will be in November 2022. Widen Lake Hazel Rd to five 11' lanes with curb gutter and 10' to 14' detached multiuse path on both sides, narrowing the path as necessary to avoid obstructions in accordance with the completed scoping sketch. Install a retaining wall on the north side on front of existing development. Relocate the existing utility poles into the buffer between the roadway and the multi-use path. Bridge improvement included to remove and replace the existing bridge over the Ten Mile Creek.** Design will be done in 2022. Construction is planned for 2024 in the adopted IFYWP with a total cost of \$2.3M.

*City Staff Contact: Miranda Carson, Community Development*

Eagle Road Signal Improvements: **Work is ongoing through the end of October.**

2021 Residential Capital Maintenance Project: ACHD's objective with maintenance projects has expanded outside pavement rehabilitation and now aiming for complete streets. The project is currently in the design phase, and construction is planned for the spring/summer of 2022. The City has requested the segments include a travel lane in each direction, on-street parking parallel to a curb with gutter and sidewalk. The project includes the following segments: 5th, Franklin to Bower; 4th, Broadway to State; State Ave, Meridian to E End; Bower, Main to East End; 4th, Washington to North End; 5th, State to N End; 2nd, Idaho to State Ave

FY22 Arterial & Collector Capital Maintenance: 95% design was distributed to the project team. The two Meridian roadway segments are Victory Rd, Black Cat Rd To Ten Mile Rd & Amity Rd, Meridian Rd to Locust Grove Rd.

Overland/Eagle Signal: Widen intersection to 7 lanes on Eagle and 9 on Overland. Project to be done in coordination with ITD. Design is indicated for 2025 in the IFYWP. All phases shown as Unfunded in the IFYWP at a cost of \$8.16M.

## ITD PROJECTS

ChindenWest Corridor: This project will result in 4 travel lanes (two in each direction). Widening will take place to the south of the existing alignment and be constructed to accommodate future widening to 6 travel lanes (three in each direction) with high capacity intersections at roads with river crossings. Project website:

[www.ITDprojects.org/ChindenWest](http://www.ITDprojects.org/ChindenWest)

ID-16 to Linder: The widening of this section of Chinden Boulevard is complete. The new roadway includes four 12-foot travel lanes, two in each direction, and a detached 10-foot pathway to the south for bicyclists and pedestrians.

Locust Grove Road to Eagle Road: Four lanes opened December 2020. These changes come with new turning movements out of the Banbury and Bristol Heights neighborhoods.

Linder Road to Locust Grove Road: All four lanes from Linder to Meridian are open. Motorists can expect periodic lane closures throughout the summer as crews continue work in the intersections and adjacent to the roadway. All remaining work will take place during off peak hours. Periodic flagger control at intersections is also anticipated in the evenings to complete stripping and landscaping. Locust Grove Road to Meridian is in the property acquisition negotiation phase. Construction is anticipated in 2022. All parties are committed to completing this work as soon as possible to achieve four uninterrupted lanes of traffic, two-in-each-direction, from Eagle to State Highway 16.

Star Road to ID-16: Construction is planned for 2023.

*City Staff Contact: Caleb Hood, Community Development*

Idaho Highway 16 extension, U.S. 20/26 to I-84: In May, the Idaho Transportation Board approved \$170 million of Transportation Expansion and Congestion Mitigation (TECM) funds for this project. Phase 2 will include the first portion of the interchange at I-84; at grade intersections at Franklin, Ustick, and US-20/26; bridges over Cherry Lane and McMillan Road with no connections; bridges over the railroad, 10 Mile Creek, 5 Mile Creek; pedestrian bridge over 5 Mile Creek east of McDermott Road; and a few improvements to the local roads on Franklin, Ustick, and McDermott. Phase 2 design and ROW are in process. Construction of Phase 2 is expected to begin in 2022 and take 2-3 years. At this point, it is too early to know the location where ITD will begin construction; likely the 2 ends will be constructed first. Project website: [www.ITDprojects.org/Idaho16](http://www.ITDprojects.org/Idaho16)

SH-69, Kuna to Meridian Traffic Study: **ITD is currently reviewing typical cross-sections and a drainage report is being developed that will determine typical cross-section widths.** ITD is studying options to improve safety, mobility and access on Idaho 69 between Kuna and Meridian. The current concept includes continuous flow intersections, median U-turn intersections, reduced conflict U-turn intersections, additional traffic lanes, and bicycle and pedestrian facilities. There is currently no funding identified in ITD's seven-year budget for the SH-69 improvements. Here is a link to the project website: <https://itdprojects.org/projects/id69corridor/>

## **PATHWAYS**

Rail with Trail: In the fall of 2012, the City applied for an \$85,000 grant to study the Rail with Trail (RWT) pathway crossing of streets (7 crossings; Black Cat to Locust Grove). Currently, there are funds for pathway construction in the Regional Transportation Improvement Plan (TIP) in 2022.

Nine Mile Creek, Linder to Meridian: The path is proposed to run from Linder to Meridian along the Nine Mile Creek drain just north of the railroad. City Parks Department is working with a consultant to negotiate easements for four (4) parcels from Linder to 8<sup>th</sup> St. The City currently has obtained all but one required easement from 8th to 3rd. We are currently pursuing acquisition of easements from 8th Street west to Linder, and from 3rd Street to Meridian Road as a future phase of this project.

Five Mile Creek Pathway: The City applied for a COMPASS grant to design and construct a portion of the Five Mile Creek pathway near the Wastewater Resource Recovery Center

(WWRRC). The City informed COMPASS that we will pursue completion of this project with local funds instead of Federal. There has been purchase and development action on the properties north of Five Mile Creek, west of the WWRRC. Additionally, ACHD has purchased a parcel to the south of the WWRRC. It appears that the bulk of this pathway will be constructed by these new owners as a condition of development. The City plans to invest in pedestrian crossings of the Nine Mile and Five Mile Drains to skirt the WWRRC and achieve continuity with existing McNelis Pathway along the Five Mile Drain.

**South Meridian Neighborhood Bicycle & Pedestrian Plan:** **The plan went in front of the ACHD Commission for adoption on October 27<sup>th</sup>. Scoping began on the 5 sub projects. Two projects are on Locust Grove Rd: Palermo Dr pedestrian crossing and bridge #1207 just south of Victory. Three projects are improvements along Stoddard Road, Victory to Overland. Design is planned for 2022, and construction is not yet programmed.** ACHD and several partners including the City coordinated to develop a plan to help improve walking and bicycling routes in South Meridian neighborhoods. Here is a link to the project site:

[http://www.achdidaho.org/Projects/proj\\_program\\_south-meridian-neighborhood-bicycle-and-pedestrian-plan.aspx](http://www.achdidaho.org/Projects/proj_program_south-meridian-neighborhood-bicycle-and-pedestrian-plan.aspx)

*City Staff Contact: Miranda Carson, Community Development*

## **MDC / OTHER PROJECTS**

**2<sup>nd</sup> Street, Broadway to Pine:** This project was submitted to COMPASS for consideration of a Project Development Grant for \$25,000. The vision for 2<sup>nd</sup> street was presented to stakeholders and the ACHD commission in separate meetings in January. Discussions are beginning on how to activate 2<sup>nd</sup> Street in Downtown Meridian to be a community place. This project will be done in coordination with the Civic Block redevelopment.

*City Staff Contact: Miranda Carson, Community Development*

**Valley Regional Transit:** The Harvest Lifestyle service has been operating since December 2016; the service is operated by Harvest Church in partnership with VRT. The Veterans Shuttle service for Meridian Veterans to schedule rides to the VA Medical Center in Boise launched May 2021. A new route from the Ten Mile Interchange area through downtown to The Village will launch in 2022. Intercounty routes 40 and 42 also serve Meridian residents. For more information visit:

<https://www.valleyregionaltransit.org/>

*City Staff Contact: Miranda Carson, Community Development*

**Ada County Transportation Action Plan:** Ada County, working with consultant Kittelson & Associates, is creating the Ada County Transportation Action Plan (TAP). The purpose of the TAP is for Ada County to identify a broad set of transportation policies, goals, and strategies for the unincorporated parts of Ada County. Here is a link to the plan webpage:

<https://adacounty.id.gov/developmentservices/transportation-action-plan/>

**Eagle Road Bike/Ped Project Development:** COMPASS has contracted with Keller Associates to further develop a bicycle and pedestrian plan for the corridor. The boundaries of the project are Overland and Chinden.

**Shared Vehicles:** **An inquiry was received from Spin; Council does not want to discuss vehicle sharing at this time.** The City does not currently have an RFP out. Meridian Downtown Business Association (MDBA) will monitor the climate and recommend back to the City when they believe micro mobility is appropriate.



TOPIC	VRT Update – FY 2021 Ridership
DATE	November 1, 2021
STAFF MEMBER	Alissa Taysom

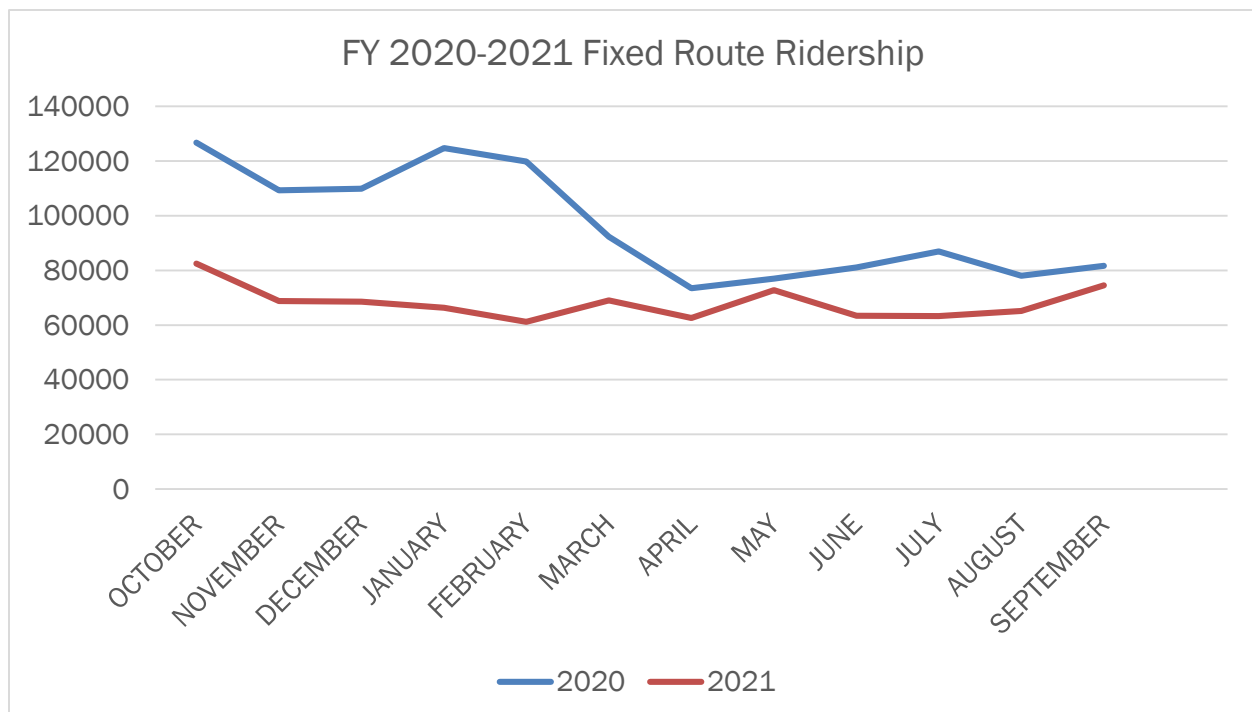
### Highlights

- Launch of the route 30 Pine delayed due to impacts of pandemic on ridership
- Ridership is no longer going down and is trending up
- Routes 40 and 42 had significant growth in September 2021
- 908 Harvest Transit boardings in September 2021
- 14 VA Shuttle boardings in September 2021

### Summary

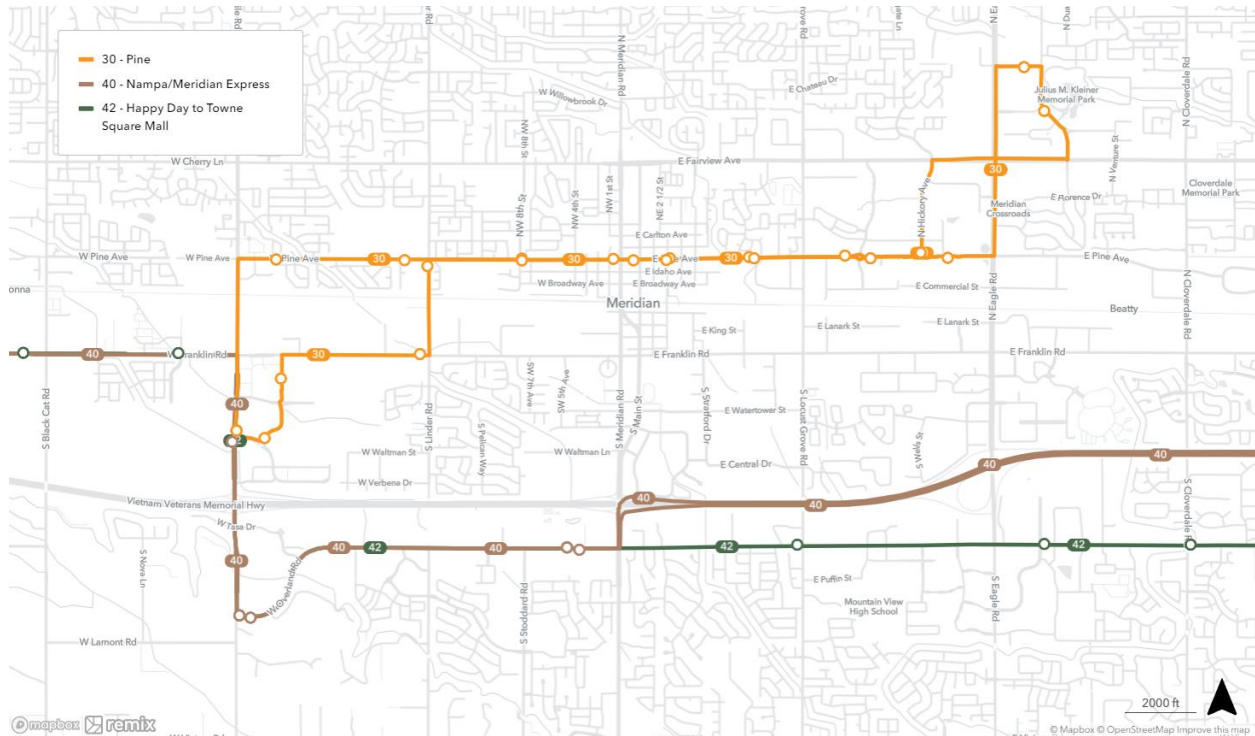
The COVID-19 pandemic caused a significant drop in ridership from pre-pandemic levels. Although ridership is still far below pre-pandemic levels, there are signs on recovery. In September 2021, VRT fixed route ridership improved by 14% over the previous month. The intercounty routes that serve Meridian (the 40 and 42) saw even more significant improvement, with 23% growth over August's.

The routes 40 and 42 carried 3,227 passengers in September, and more than 8,300 in the fourth quarter of FY 2021. Approximately 20% of the activity on the routes 40 and 42 occurred in Meridian.

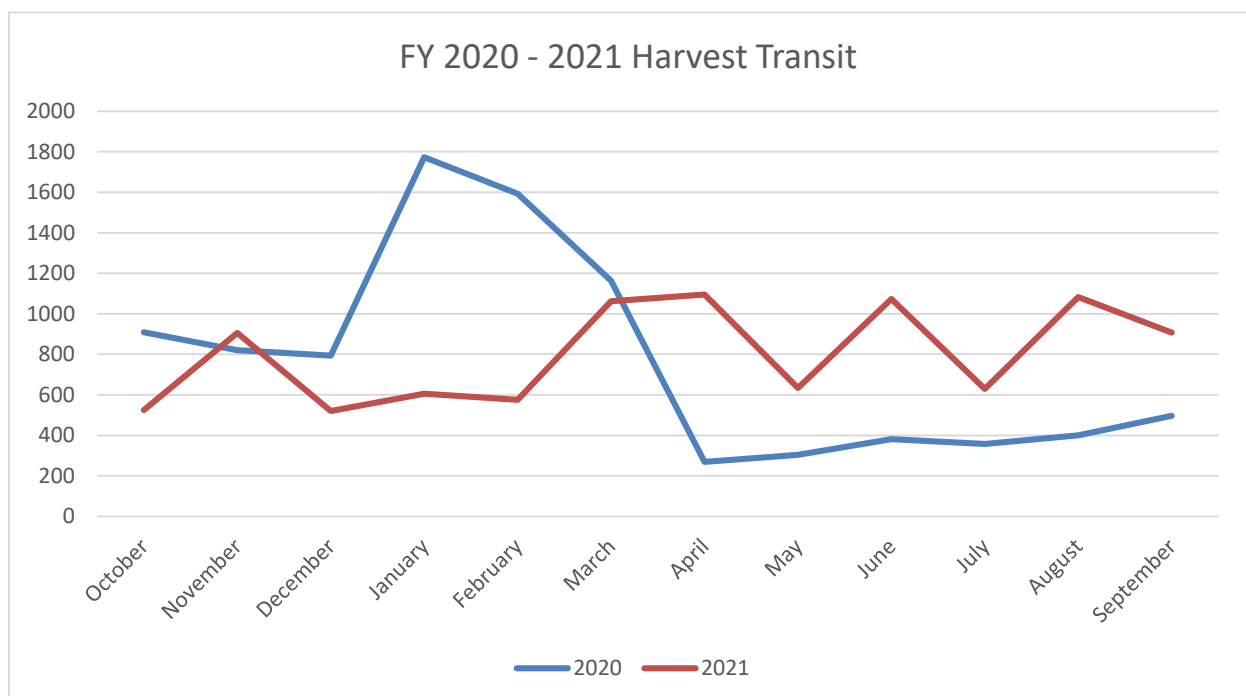




The slow recovery has led to the delay in the start of the Route 30 Pine, however the last few months of growth are encouraging signs, which are being considered as discussion goes forward on when the service will officially start.



In September 2021, Harvest Transit had 908 total riders, a decrease from the month before. However, from FY 2020 to FY 2021, ridership is slowly recovering and has increase over the same month the previous year.



The Meridian VA Shuttle launched in May 2021, and is growing slowly. In May, the VA shuttle had a total of 6 trips, and in September of this year, the total number of trips was 14.

### **More Information**

**For detailed information contact:** Alissa Taysom, Associate Planner, 208-258-2717, [ataysom@valleyregionaltransit.org](mailto:ataysom@valleyregionaltransit.org).



# MEMO TO TRANSPORTATION COMMISSION

## Agenda Topic on the Transportation Commission Agenda

**From:** Miranda Carson, Coordination Planner      **Meeting Date:** November 1, 2021  
**Presenter:** Miranda Carson      **Estimated Time:** 15 minutes  
**Topic:** Chinden Access West of Hwy-16

The intent of this memo is to solicit input from the Transportation Commission regarding access to Chinden west of Hwy-16. The Star Road to Hwy-16 segment is currently in design as part of the ChindenWest project through ITD with construction anticipated to begin in 2023. A recent conversation between City and ITD staff, staff was asked if the City has a preference for/against midblock access. Currently there is midblock access along Chinden east of Hwy-16.

The City of Meridian Comprehensive Plan includes a policy to reduce the number of existing access points onto arterial streets by using methods such as cross-access agreements, access management, and frontage/backage roads, and promoting local and collector street connectivity (Policy 6.01.02B). Further, Meridian City Code 11-3H-42b prohibits direct lot access to highways except at the mid-mile location.

Residential collectors are intended at the half mile per several plans including the City of Meridian Comprehensive Plan, ACHD Master Street Map, and City of Star Comprehensive Plan. Star has indicated a preference for access at the mile (section line) and half mile. Staff would like the TC to comment on the proposed access in this two-mile segment of the corridor, between Can-Ada Road and McDermott/Hwy-16 to confirm this previous direction.

### Illustrative Framework Plan

#### Use Type

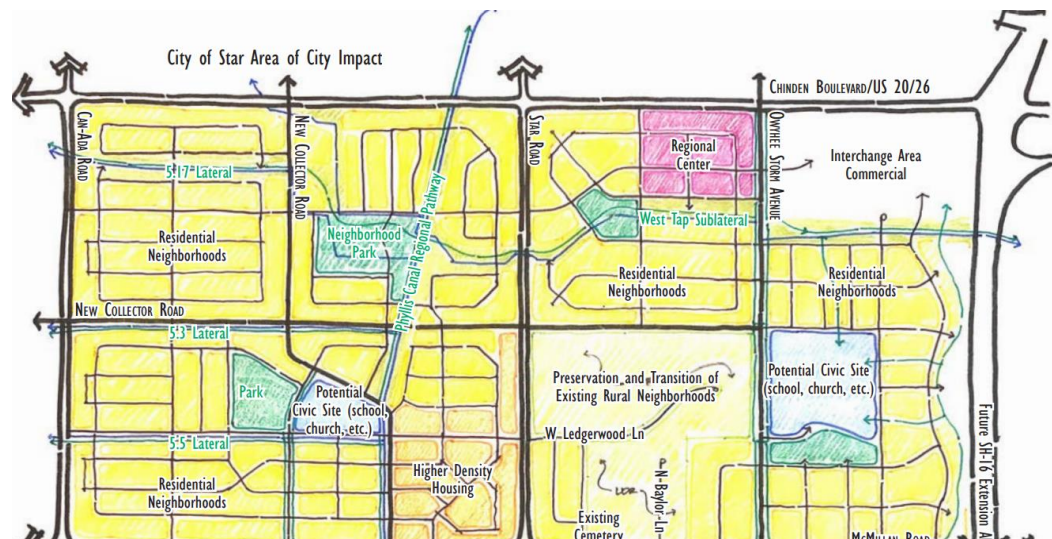
- Medium Density Neighborhoods
- Medium-High Density Neighborhoods
- Industrial Area
- Retail/Office Areas
- Business Park
- Potential School/Church
- Parks and Greenways
- Historic Site

#### Roadways and Pathways

- Highways and Arterials
- Collectors
- Local Streets
- Pathways

Refer to Star/McMillan Schematic Concept for additional detail

Note: The Illustrative Framework Plan is conceptual in nature. Final platting and design may differ.



*Excerpt from Fields Subarea Plan (Plan to be considered by Council December 7, 2021)*





# MEMO TO TRANSPORTATION COMMISSION

## Agenda Topic on the Transportation Commission Agenda

**From:** Miranda Carson, Coordination Planner      **Meeting Date:** November 1, 2021  
**Presenter:** Miranda Carson      **Estimated Time:** 10 minutes  
**Topic:** Staff Communications

Below are summaries of relevant correspondences received by City Staff since the last TC meeting.

### COMPASS Charter School Traffic

The City received comments regarding concerns with traffic on Black Cat between Pine and Franklin during the morning arrival and afternoon dismissal. This is the location of the new Compass Charter School. Correspondence on the issue was included in the October Staff Communications memo.

I received this update from Kendall Kemmer at ACHD:

I found out yesterday that our Design Department conducted an evaluation of Black Cat Road and determined that simply widening the pavement for a southbound left turn lane, is not possible. The primary issue is the location of the canal, on the west side of Black Cat, but other issues as well. With this being the case, the decision was made that the addition of a left turn lane would need to be part of the future widening project for Black Cat Road. The road is designated five-lane in the future, but not included in the current Five-Year Work Plan (FYWP). As you probably know the FYWP is managed by ACHD's Planning Department. I would recommend contacting them to find out more details on future work. Moving this project up in the City of Meridian's annual prioritization list may be something for consideration as well. Even after it gets added to the Five-Year Work Plan it would still need to go through the design and right-of-way acquisition process before construction could begin. Since this roadway has a railroad crossing additional time would be needed to get an agreement signed with the railroad.

Sidewalks – As you mentioned this school is in more of a rural environment and not directly adjacent to the existing subdivisions. As such, it is understandable that the sidewalk network is not complete, but also need to remember that this is where the school chose to build. As a non-traditional school the vast majority of the students will get to/from school, by means other than walking or biking, but based on your observations there are some who chose to walk. If you would like to request new sidewalks, I will again direct you to ACHD's Planning Department. They are in charge of the Community Program, which includes requests for sidewalks. Once again this is something that the city may want to consider including in the annual request list.

Extruded Curbs – I do not believe that ACHD uses extruded curbs anymore, and don't think that we have done so for a number of years. I think issues with ADA may be one of the reasons why we quit building them. Once again, I would check with Planning to see if the Commission would consider the use of this type of curbing.

I also received this update from Lieutenant Harper:

Here is the update from Sgt Gonzales who had Officer Haustveit monitor the morning and afternoon traffic at Compass:

A few things that we knew immediately were:

- The traffic challenges at Compass were for short periods of time much like any other school within the City.
- Compass has always work at trying to keep traffic flow moving for pick-up and drop off. They had developed 2 lanes for traffic for this purpose instead of only 1.
- Compass has used the proper layout for traffic and parking. In general, the recommendation is to have 4 different areas that need to be kept separate in order to increase safety and Compass follows that model:
  - Student parking—east end of the campus off Aviator
  - Pickup and Drop off—in front of the school using 2 lanes to increase traffic flow
  - Visitor and teach parking—parking lot in front of the school
  - Busing—on the south end of campus with access off of Franklin Rd.
- Through continual communication with the school and other schools, we knew that busing has been a large challenge due to the lack of bus drivers the number of parent's dropping off students has increased. Specifically, for Compass the estimate is an estimated 150 additional cars currently due to the busing issues.

I had our liaison for Compass, Officer Jared Haustveit, to look at the school and see what could be done differently. He visited the school in the morning when students were being dropped off and at the end of the day. I stopped by the school and the leadership team happened to be meeting, so I was able to speak to the entire leadership team of the school. The leadership team has clearly been doing everything possible to keep traffic moving and are open to suggestions. A few things that I learned by meeting with the leadership team were:

- Compass developed an app to help connect parents with each other to increase carpooling opportunities.
- A large push has been made to get the adequate number of buses within their busing system of 7 buses. They had dropped to 4 buses are currently up to 6 and will be getting their 7<sup>th</sup> bus soon.
- The school has repeatedly communicated with parents trying identifying the traffic flow plan and offering suggestions to reduce the traffic congestion.
- A traffic study was conducted when the building of the school was being permitted. The school followed the recommendations of the traffic study and were approved. The school did have to provide the funding to have Aviator Street but were not required to make any other traffic control improvements.
- The 10-acres east of the school has been purchased for residential development which will lead to more traffic using Aviator.

- The construction of the neighborhood just north of Aviator will only be accessible off Aviator so additional vehicles will be using the same access point as the school.

The traffic concerns observed are:

- Volume of vehicles
- Backup of traffic in both directions on Black Cat at Aviator
- Backup on Franklin Rd., but not much

The leadership team of Compass had the following recommendations/requests:

- Improvement of railroad crossing on Black Cat Rd. The stop sign at this location creates backup and congestion on Black Cat all the way to Franklin Rd.
- Right-In and Right-Out into the Compass campus; however, the leadership team did note that without improving the rail crossing the Right-In and Right-Out changes would not really improve the current situation. The backup for southbound traffic is when vehicles stop to turn east (left) onto Aviator from Black Cat Rd.

Conclusion: I do not believe Black Cat Rd is planned to be improved any time soon to possibly alleviate some traffic congestion. I would imagine the problem will continue to deteriorate until Black Cat Rd is improved. Additionally, residential construction continues to increase in the area. I do not have any specific recommendations that would easily alleviate the traffic issues in the area. I knew the school would stay on top of this issue, but when I had the opportunity to meet with them it was clear they were doing everything possible to keep traffic flowing safely on their campus.

At this time there is not any other action to be taken until we see the draft IFYWP. When the draft is distributed for comments, we can then determine if we want to send a request this project be advanced. I will monitor that progress.

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## **Development Transportation Improvements**

On October 1<sup>st</sup> Chair Walter Steed inquired about a traffic signal and turn lanes being installed at McDermott and Ustick. I responded that these are projects that are being completed through the Owyhee High School development conditions. These types of projects are not on the ACHD webpage or provided through their weekly updates.

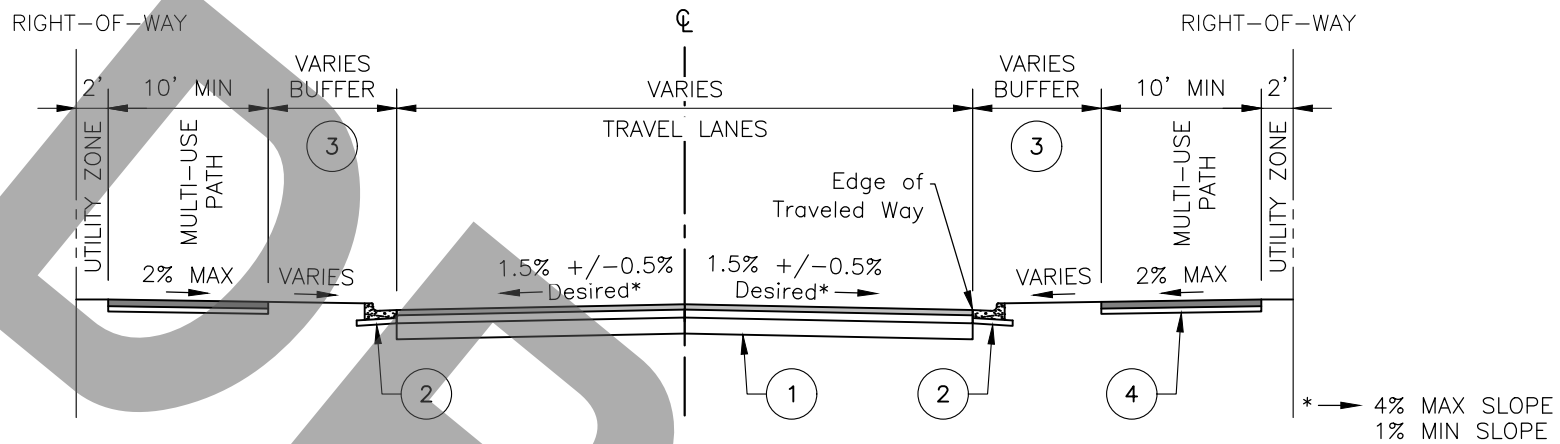
After discussion he and I agreed that a running list of projects required by new developments may be a good task for the Development Review subcommittee to maintain.

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## **ACHD Bike and Pedestrian Improvements Guidelines**

City staff requested clarification from ACHD on when bike lanes, multiuse pathways, and sidewalks would be designed based on context. ACHD sent the attached guides.

## MULTI-USE PATH



### NOTES

- 1 MATERIAL SECTIONS WILL BE DETERMINED BY ACHD DURING DESIGN. REFER TO ACHD POLICY MANUAL FOR ADDITIONAL INFORMATION.
- 2 STANDARD 6" VERTICAL CURB AND GUTTER PER ACHD STANDARD DRAWING SD-701, SHOWN. SPECIFIC CURB TYPES TO BE DETERMINED BY ACHD DURING DESIGN. (REFERENCE 1)
- 3 ROADSIDE BUFFER IS MEASURED FROM EDGE OF TRAVELED WAY TO FRONT OF MULTI-USE PATHWAY. REFER TO TABLE 1, FOR BUFFER WIDTHS BASED ON LEVEL OF TRAFFIC STRESS (LTS). LTS 1 OR 2 PREFERRED. MINIMUM 3' BUFFER BETWEEN CURB AND MULTI-USE PATHWAY. BUFFER MATERIAL TO MEET CURRENT ADA DETECTABILITY STANDARDS.
- 4 MATERIAL SECTION WILL BE DETERMINED BY ACHD DURING DESIGN.

TABLE 1: MULTI-USE PATHWAY BUFFER WIDTH PER LEVEL OF TRAFFIC STRESS

TOTAL TRAVEL LANES	TOTAL BUFFER WIDTH (INCLUDES CURB & GUTTER, SHOULDERS, PARKING, LANDSCAPING, ETC.)			
	<5'	5'-10'	11'-14'	15'+
1-2	LTS 2	LTS 2	LTS 1	LTS 1
3	LTS 3	LTS 2	LTS 1	LTS 1
4-5	LTS 4	LTS 3	LTS 2	LTS 1
6+	LTS 4	LTS 4	LTS 3	LTS 2

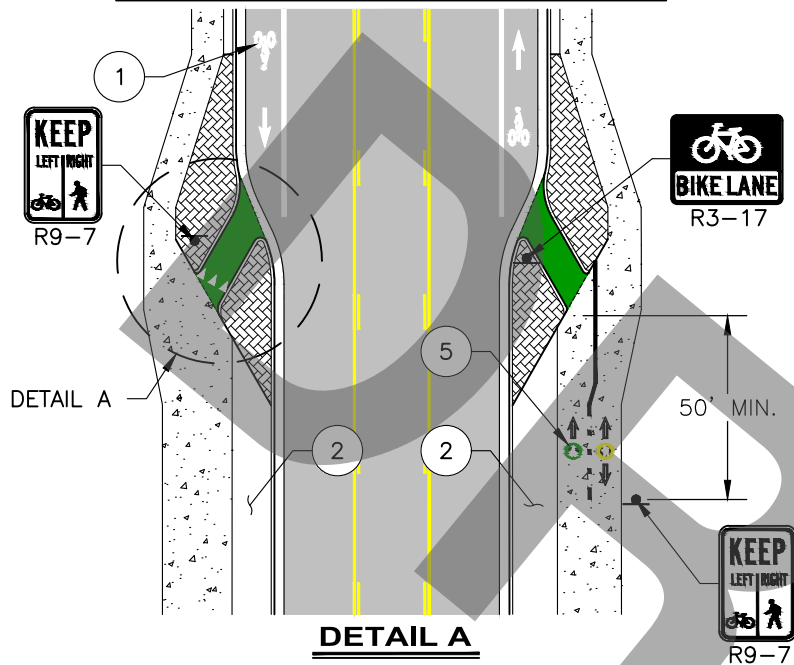
SHEET 1 OF 7

EXHIBIT NO.  
MUP-01

MULTI-USE PATH EXAMPLE APPLICATIONS  
TYPICAL SECTIONS

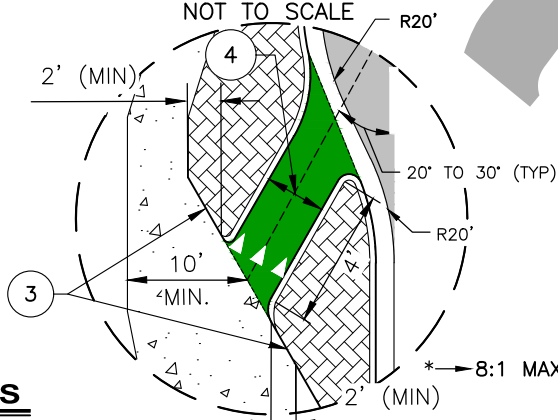


## MULTI-USE PATH TRANSITION TO CONVENTIONAL BIKE LANE



**DETAIL A**

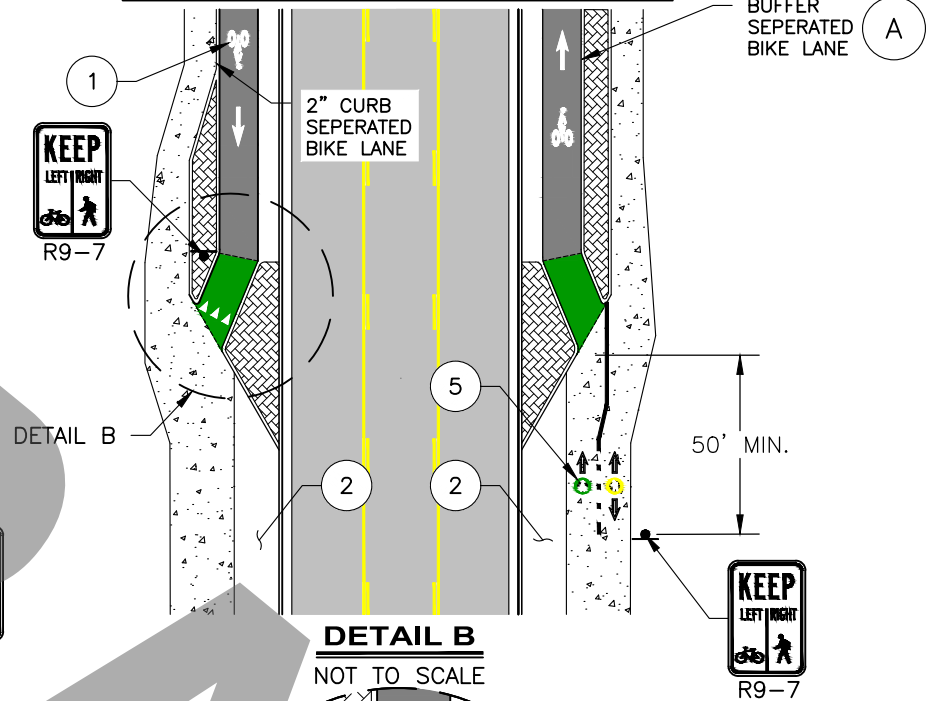
NOT TO SCALE



## NOTES

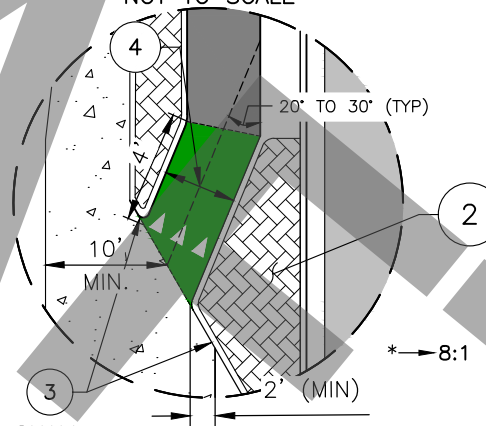
- 1 BIKE LANE MARKING PER ACHD STANDARD DRAWING TS-1113.04. MARKING SHALL BE PLACED IMMEDIATELY BEFORE AND AFTER A BIKE TRANSITION RAMP.
- 2 BUFFER MATERIAL TO BE DETECTABLE, PER CURRENT ADA DETECTABILITY STANDARDS.
- 3 INTRODUCE GEOMETRIC DEFLECTION OF DETECTABLE BUFFER FOR POSITIVE GUIDANCE ALONG PEDESTRIAN DESIRABLE PATH.
- 4 MATCH RECEIVING BIKE FACILITY WIDTH, 5' MINIMUM.
- 5 LANE ASSIGNMENT PAVEMENT MARKINGS ACCOMPANIED BY R9-7 SIGN MAY BE USED AT TRANSITION LOCATIONS. SEE PORTLAND BUREAU OF TRANSPORTATION (PBOT) STANDARD DRAWING P-435 FOR RECOMMENDED PAVEMENT MARKING DETAILS (REFERENCE 8).
- A THE SAME PRINCIPALS APPLY TO BOTH CURB SEPARATED AND BUFFER SEPARATED RAISED BIKE LANES.

## MULTI-USE PATH TRANSITION TO RAISED BIKE LANE



**DETAIL B**

NOT TO SCALE



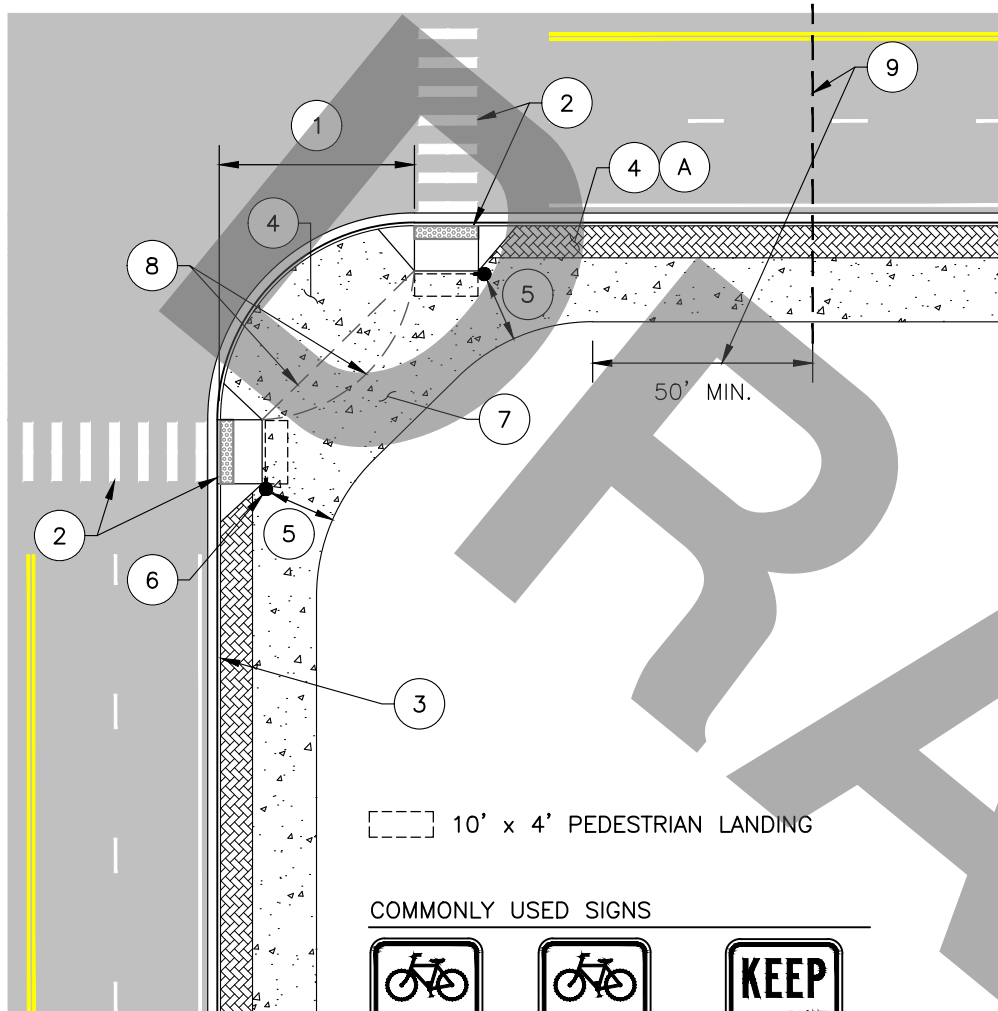
SHEET 2 OF 7

EXHIBIT NO.  
MUP-02

MULTI-USE PATH EXAMPLE APPLICATIONS  
TRANSITION TO BIKE LANE



## ELEMENTS OF A MIXED-USE PATH INTERSECTION (SIGNALIZED)



10' x 4' PEDESTRIAN LANDING

### COMMONLY USED SIGNS



R9-5



R9-6



R9-7



R10-4B(L)  
MOD



R10-15(R)  
MOD

### NOTES

- 1 6' TO 16' FOR 35 TO 45 MPH MAINLINE. 16' TO 24' FOR 45 TO 50 MPH MAINLINE. MINIMUM SETBACK FROM FACE OF CURB TO MUP CROSSING TO IMPROVE VISIBILITY FOR VEHICLES, PEDESTRIANS, AND BICYCLISTS. 6' MINIMUM SETBACK WHEN ADJACENT TO RIGHT TURN LANE. (REFERENCE 3, 4, 5)
- 2 WIDTH OF CROSSWALK AND CURB RAMP OPENING SHALL BE EQUAL TO THE WIDTH OF THE MULTI-USE PATH.
- 3 6" VERTICAL CURB & GUTTER PER ACHD STANDARD DRAWING SD-701.
- 4 AREA MAY BE UTILIZED FOR SIGNAL EQUIPMENT AND OTHER TRAFFIC CONTROL DEVICES.
- 5 10 FT MINIMUM FROM FACE OF SIGNAL OR BUTTON POLE BASE TO BACK OF PATHWAY FOR ZONE MANEUVERABILITY.
- 6 PEDESTRIAN PUSH BUTTON POLE EQUIPPED WITH APS PER ACHD STANDARD DRAWING TS-1106.02.
- 7 AREA TO REMAIN CLEAR AND FREE FROM OBSTRUCTIONS FOR MULTI-USE MIXING ZONE AND QUEUE STORAGE.
- 8 CURB (OR OTHER ADA DETECTABLE SURFACE FEATURE) MAY BE UTILIZED IN THIS AREA FOR CHANNELIZATION. IF UTILIZED, MINIMUM CLEAR SPACE BETWEEN CURB AND BACK OF PATHWAY SHALL BE EQUAL TO THE WIDTH OF THE MULTI-USE PATHWAY.
- 9 SEE SHEET MUP - 2 FOR MULTI-USE PATHWAY TO BIKE LANE TRANSITIONS.
- A SIGNAGE IS CONSIDERED CONTEXT SENSITIVE TO EACH PROJECT AND SPECIFIC SIGNAL EQUIPMENT LAYOUT. MUTCD SIGNS TO BE CONSIDERED INCLUDE R9-5, R9-6, R9-7, R10-4B(L/R) MOD, R10-15 (MOD).

SHEET 3 OF 7

Item 7.

EXHIBIT NO.

MUP-03

MULTI-USE PATH EXAMPLE APPLICATIONS

SIGNALIZED INTERSECTION



22

## ELEMENTS OF A ROUNDABOUT MULTI-USE PATH

EXAMPLE CORNER WITH  
PARALLEL RAMP

EXAMPLE CORNER WITH PERPENDICULAR RAMP

### NOTES

- 1 MULTI-USE PATH TRANSITION RAMP SHALL NOT EXCEED 12:1 SLOPE.
  - 2 10' BY 4' MINIMUM PEDESTRIAN LANDING FOR PUSH BUTTON. LANDING AREA NOT NECESSARY AT LOCATIONS WHERE RRFBs AREN'T WARRANTED.
  - 3 SLOPE OF AREA ADJACENT TO PUSH BUTTON SHALL NOT EXCEED 2%.
  - 4 PARALLEL RAMP.
  - 5 CROSSING WIDTH IN SPLITTER SHALL MATCH THE WIDTH OF THE MULTI-USE PATHWAY.
- A SIGNAGE IS CONSIDERED CONTEXT SENSITIVE TO EACH PROJECT AND SPECIFIC SIGNAL EQUIPMENT LAYOUT. MUTCD SIGNS TO BE CONSIDERED INCLUDE R9-5, R9-6, R9-7, R10-4B(L/R) MOD, R10-15 (MOD). SEE SHEET MUP-03 FOR SIGN FACES.

SHEET 4 OF 7

EXHIBIT NO.  
MUP-04

MULTI-USE PATH EXAMPLE APPLICATIONS  
ROUNDABOUT

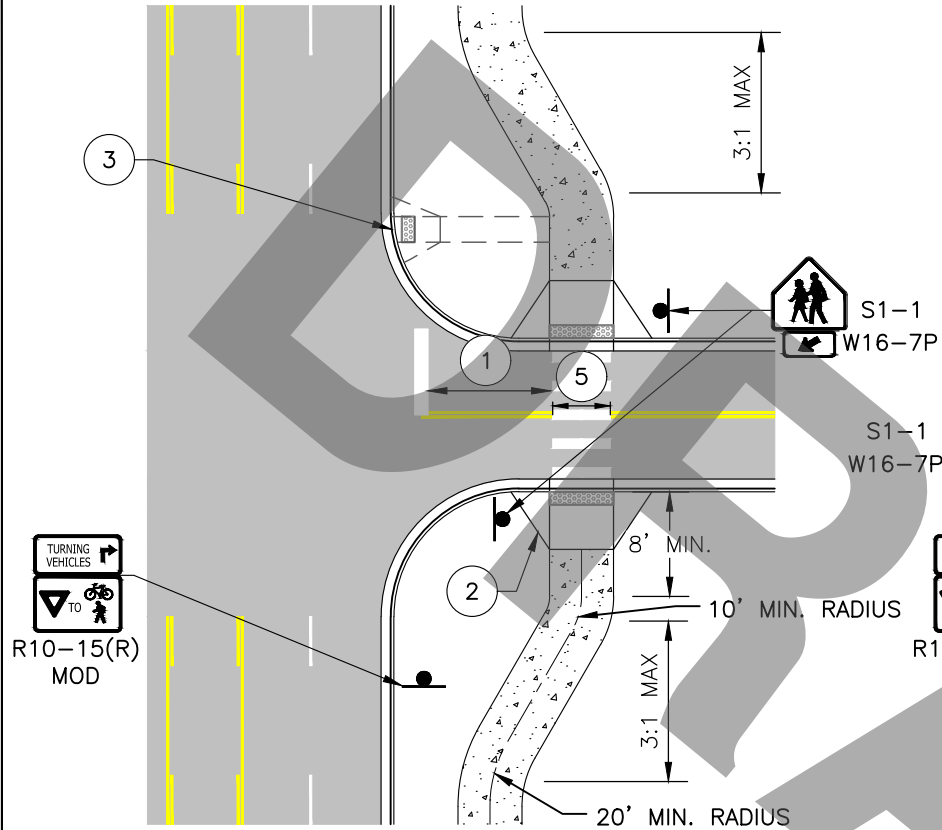


23



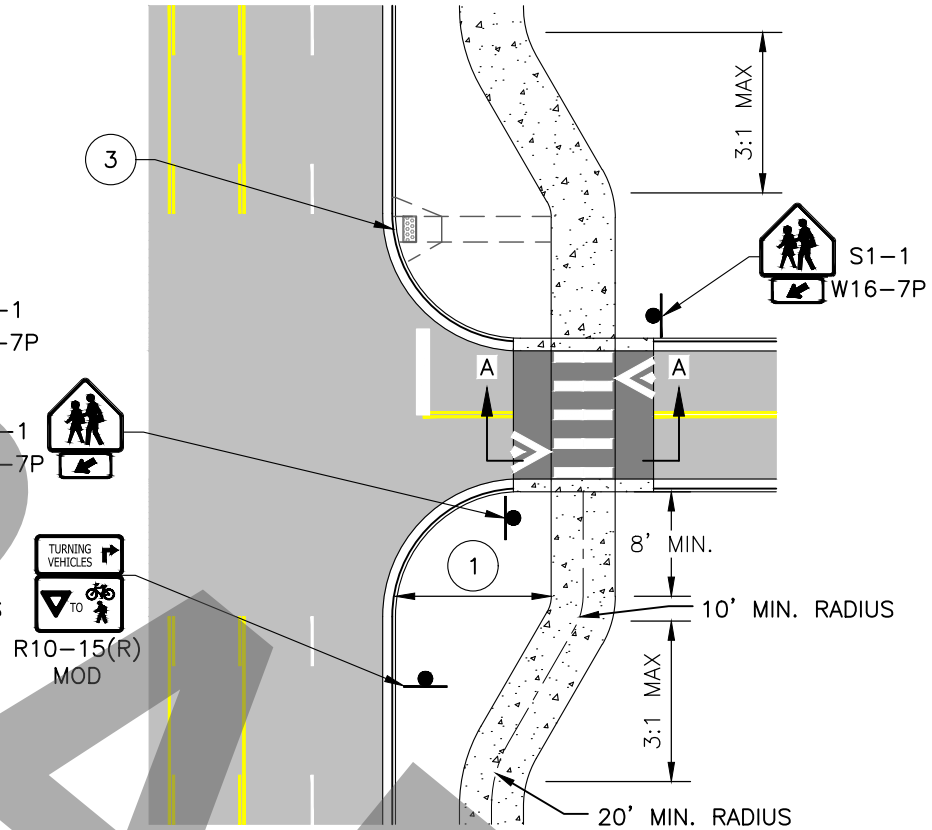
## AT-GRADE SIDE STREET CROSSING

FOR MAINLINE SPEEDS OF 40 MPH OR GREATER

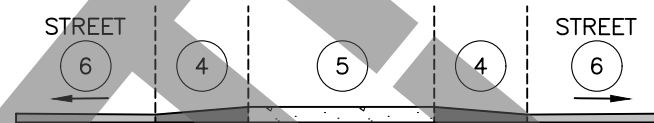


## RAISED SIDE STREET CROSSING

FOR MAINLINE SPEEDS OF 40 MPH OR GREATER



### DETAIL A



### NOTES

- 1 16' MINIMUM SETBACK FROM STOP BAR. MINIMUM SETBACK TO BIKE CROSSING TO IMPROVE VISIBILITY FOR VEHICLES, PEDESTRIANS, AND BICYCLES. PROVIDES SPACE FOR 1 VEHICLE BETWEEN STOP BAR AND CROSSING.
- 2 MULTI-USE PATH TRANSITION RAMP SHALL NOT EXCEED 8:1 SLOPE.
- 3 WHEN CROSSING OF MAINLINE IS REQUIRED, BOTH MAINLINE PEDESTRIAN RAMPS SHALL BE DESIGNED TO ACCOMMODATE THE FULL WIDTH THE MUP. PEDESTRIAN RAMP SHALL MEET CURRENT ADA STANDARDS.

- 4 APPROACH RAMP AND DEPARTURE RAMP SHALL NOT EXCEED 12:1 SLOPE. SPEED HUMP MARKINGS SHALL BE USED ON TRANSITION RAMPS.
- 5 CROSSING WIDTH SHALL BE EQUAL TO THE MUP WIDTH. THE CROSSING SHALL MEET ADA REQUIREMENTS.
- 6 STORMWATER FACILITIES SHALL BE CONSIDERED ON A CASE BY CASE BASIS. RAISED CROSSING IS IDEAL WHEN SIDE STREETS/APPROACHES SLOPE AWAY FROM THE MAINLINE.

SHEET 5 OF 7

EXHIBIT NO.  
MUP-05

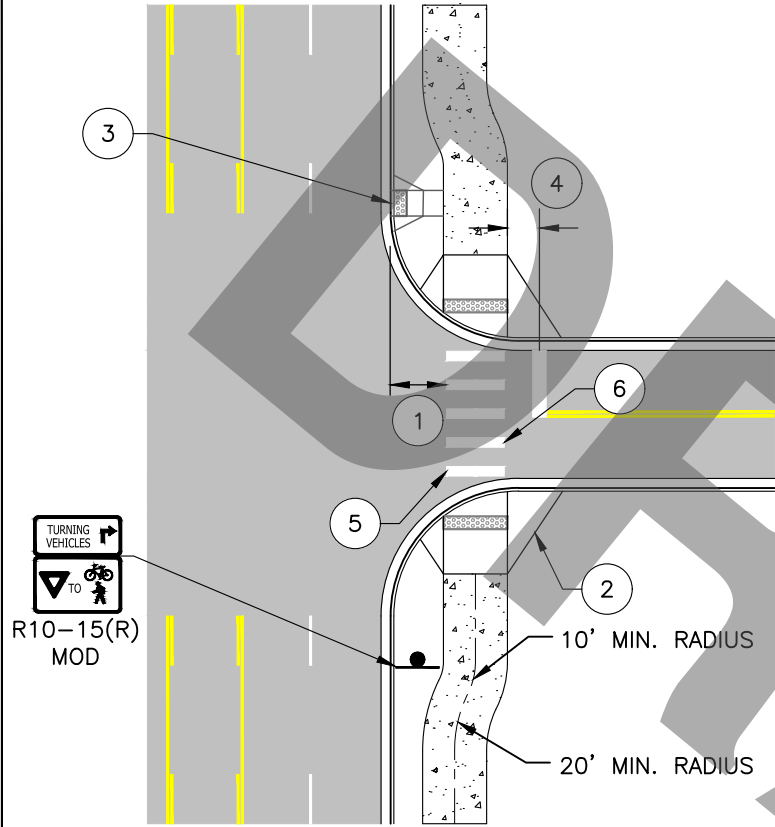
MULTI-USE PATH EXAMPLE APPLICATIONS  
UNSIGNALIZED SIDE STREET CROSSINGS





# AT-GRADE SIDE STREET CROSSING

FOR MAINLINE SPEEDS OF 35 MPH OR LESS



## NOTES

- 1 6' MINIMUM SETBACK FROM FACE OF CURB. MINIMUM SETBACK TO BIKE CROSSING TO IMPROVE VISIBILITY FOR VEHICLES, PEDESTRAINS, AND BICYCLES. (REFERENCE 3, 4)
- 2 MULTI-USE PATH TRANSITION RAMP SHALL NOT EXCEED 8:1 SLOPE.
- 3 WHEN CROSSING OF MAINLINE IS REQUIRED, PEDESTRIAN RAMP SHALL MEET CURRENT ADA STANDARDS AND DIMENSIONS SHALL BE PER THE PLANNED USE OF THE DESTINATION SIDE.
- 4 4' MINIMUM SETBACK FROM CROSSWALK PER ACHD STANDARD DRAWING TS 1112.03.
- 5 10' MINIMUM WIDTH AT SIDE STREET CROSSINGS. THE CROSSING SHALL MEET ADA REQUIRMENTS.
- 6 AT GRADE CROSSING SHOWN, RAISED CROSSINGS TO BE CONSIDERED ON A CASE BY CASE BASIS. RAISED CROSSING SHALL BE PER DETAIL A ON SHEET MUP-05.

SHEET 6 OF 7

EXHIBIT NO.  
MUP-06

MULTI-USE PATH EXAMPLE APPLICATIONS  
UNSIGNALIZED SIDE STREET CROSSINGS



## REFERENCES FOR MULTI-USE PATHWAYS

1. ADA COUNTY HIGHWAY DISTRICT. "2017 ACHD SUPPLEMENT TO THE 2017 ISPMC." DECEMBER 2017, [HTTPS://WWW.ACHDIDAHO.ORG/DOCUMENTS/ENGINEERING/ISPMC/2017\\_ISPMCSUPPLEMENTS.PDF](https://www.achdidaho.org/documents/engineering/ISPMC/2017_ISPMCSUPPLEMENTS.PDF).
2. CITY OF WEST LINN PUBLIC WORKS DEPARTMENT. SEPARATED BIKE PATH AT INTERSECTION STANDARD DRAWINGS. REVISED FEBRUARY 2019, [HTTPS://WESTLINNOREGON.GOV/PUBLICWORKS/STANDARD-DRAWINGS](https://westlinnoregon.gov/publicworks/standard-drawings).
3. MASSACHUSETTS DEPARTMENT OF TRANSPORTATION. "SEPARATED BIKE LANE PLANNING & DESIGN GUIDE." 2015, [HTTPS://WWW.MASS.GOV/LISTS/SEPARATED-BIKE-LANE-PLANNING-DESIGN-GUIDE](https://www.mass.gov/lists/separated-bike-lane-planning-design-guide).
4. NATIONAL ASSOCIATION OF CITY TRANSPORTATION OFFICIALS. "URBAN BIKEWAY DESIGN GUIDE ANNOTATED PLANS." APRIL 2011.
5. U.S. DEPARTMENT OF TRANSPORTATION FEDERAL HIGHWAY ADMINISTRATION. "SEPARATED BIKE LANE PLANNING AND DESIGN GUIDE." MAY 2015.
6. NCHRP REPORT 834 CROSSING SOLUTIONS AT ROUNDABOUTS AND CHANNELIZED TURN LANES FOR PEDESTRIANS WITH VISION DISABILITIES, JANUARY 2017.
7. NCHRP REPORT 672, ROUNDABOUTS, A GUIDE BOOK, 1ST AND 2ND EDITIONS, 2010.
8. PORTLAND BUREAU OF TRANSPORTATION. "STANDARD DRAWING P-453". DECEMBER 2017. <https://www.portland.gov/transportation/engineering/standard-drawings>
9. UNITED STATES ACCESS BOARD. "R304.5.1.2 SHARED USE PATHS". 2013 <https://www.access-board.gov/files/prowag/PROW-SUP-SNPRM-2013.pdf>
10. NCHRP GUIDE FOR LOW SPEED MULTIMODAL ROADWAYS, 2018.

SHEET 7 OF 7

Item 7.

EXHIBIT NO.

MUP-07

MULTI-USE PATH EXAMPLE APPLICATIONS

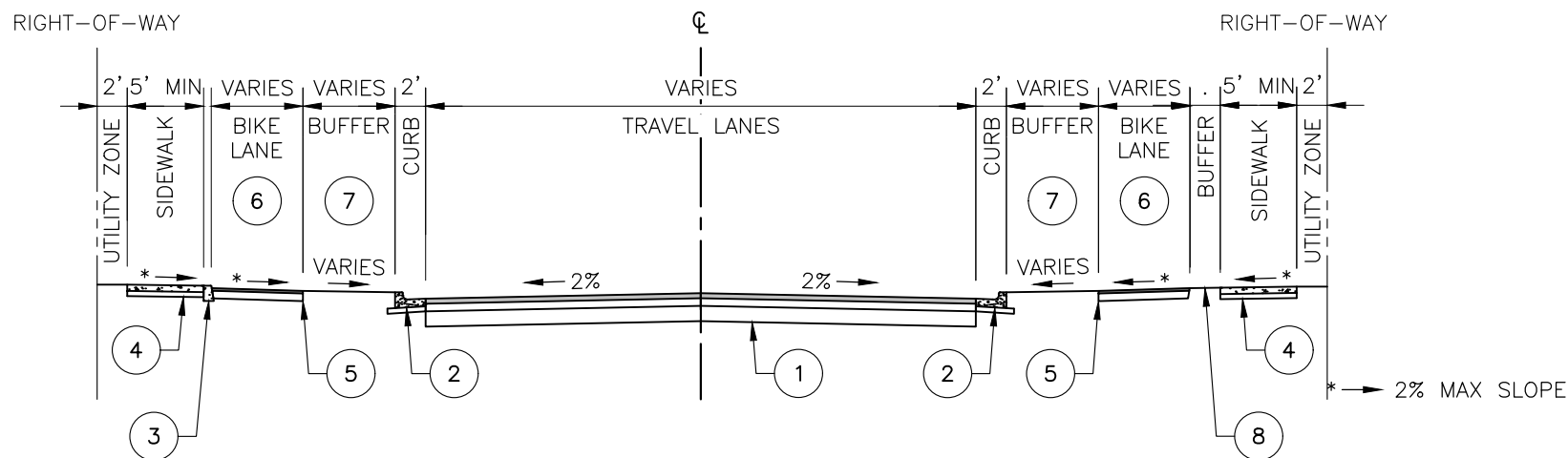
REFERENCES



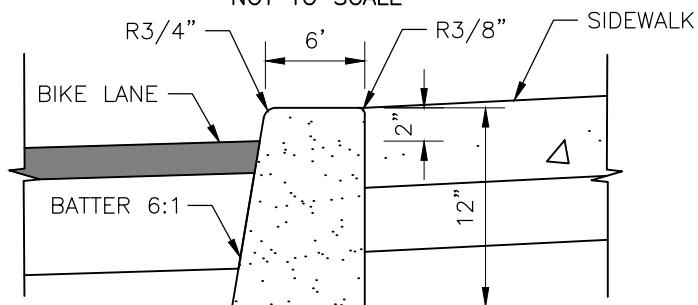
26

## CURB SEPARATED RAISED BIKE LANE

## BUFFER SEPARATED RAISED BIKE LANE



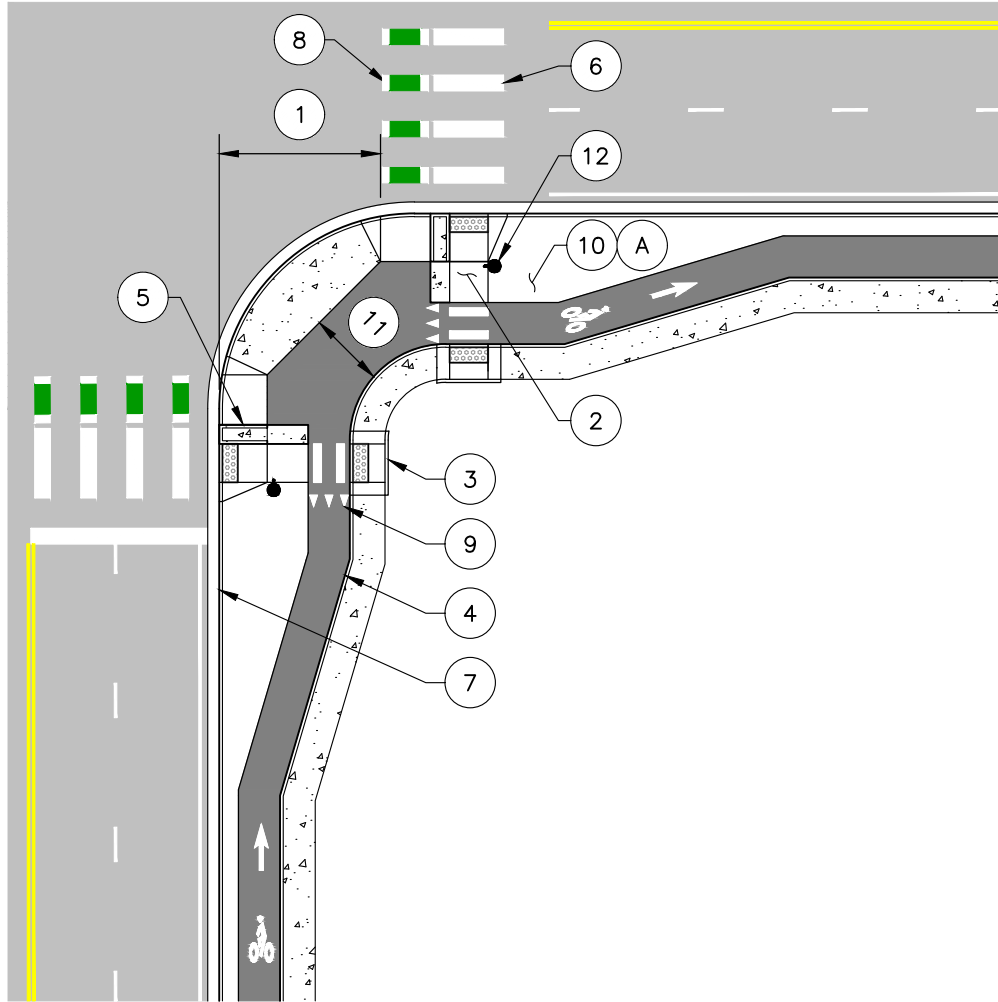
**DETAIL A**  
**NOT TO SCALE**



## NOTES

- 1 MATERIAL SECTIONS WILL BE DETERMINED BY ACHD AT THE TIME OF APPLICATION. REFER TO ACHD POLICY MANUAL FOR ADDITIONAL INFORMATION.
- 2 STANDARD 6" VERTICAL CURB AND GUTTER PER ACHD STANDARD DRAWING SD-701.
- 3 MODIFIED VERTICAL CURB WITH 2" REVEAL. REFER TO DETAIL A, THIS SHEET. 2" CURB REVEAL PROVIDES ADA DETECTABILITY.
- 4 STANDARD CONCRETE SIDEWALK PER ACHD STANDARD DRAWING SD-709.
- 5 HOT MIX ASPHALT IS THE PREFERRED SURFACE MATERIAL FOR BIKE LANES. MATERIAL SECTION WILL BE DETERMINED BY ACHD DURING DESIGN.
- 6 6.5' BIKE LANE DESIRED, 5' MINIMUM.
- 7 ROADSIDE BUFFER VARIES. 3' MINIMUM WIDTH TO ALLOW FOR SIGNAGE AND OTHER ROADSIDE FEATURES.
- 8 2' MINIMUM WIDTH FOR SIDEWALK BUFFER. CLEAR DELINEATION BETWEEN THE SIDEWALK AND BIKE LANE, INCLUDING ADA DETECTABILITY REQUIREMENTS, SHALL BE PROVIDED.

# **ELEMENTS OF A PROTECTED INTERSECTION (SIGNALIZED) CURB SEPARATED RAISED BIKE LANE**

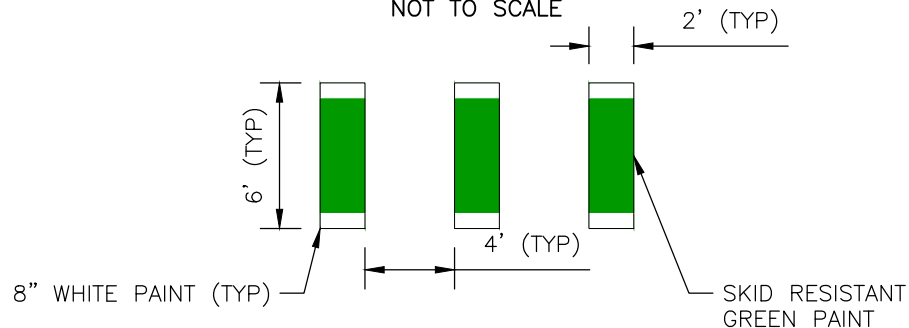


## **NOTES**

- 1 16' MINIMUM SETBACK FROM FACE OF CURB TO BIKE CROSSING TO IMPROVE VISIBILITY FOR VEHICLES, PEDESTRIANS, AND BICYCLISTS. INCREASED SETBACK IS DESIRABLE TO PROVIDE SPACE FOR A YIELDING VEHICLE WITHOUT BLOCKING THROUGH TRAFFIC.
- 2 ADA COMPLIANT PEDESTRIAN LANDING AREA, 4FT BY 4' MINIMUM.
- 3 PARALLEL RAMP, 2" DROP TO BIKE LANE LEVEL.
- 4 2" VERTICAL CURB. REFER TO DETAIL A, SHEET 1.
- 5 COMBINATION PERPENDICULAR CURB RAMP WITH 3' VERTICAL CURB SEPARATOR. 6" DROP TO ROADWAY GRADE.
- 6 CROSSWALK MARKINGS PER ACHD STANDARD DRAWING TS-1112.03.
- 7 6" VERTICAL CURB & GUTTER PER ACHD STANDARD DRAWING SD-701.
- 8 BIKE LADDER MARKINGS PER DETAIL A, THIS SHEET.
- 9 YIELD MARKINGS PER ACHD STANDARD DRAWING TS-1113.06.
- 10 AREA MAY BE UTILIZED FOR SIGNAL EQUIPMENT AND OTHER TRAFFIC CONTROL DEVICES.
- 11 10 FT MINIMUM TO PROVIDE QUEUE STORAGE AND BIKE MIXING ZONE MANEUVERABILITY.
- 12 PEDESTRIAN PUSH BUTTON POLE EQUIPPED WITH APS PER ACHD STANDARD DRAWING TS-1106.02.
- A BIKE DETECTION DEVICES INCLUDE PUSH BUTTONS, LOOP DETECTORS, CAMERAS, OR RADAR. DETECTION PREFERENCE TO BE DETERMINED BY ACHD DURING PROJECT DEVELOPMENT.

## **DETAIL A**

NOT TO SCALE



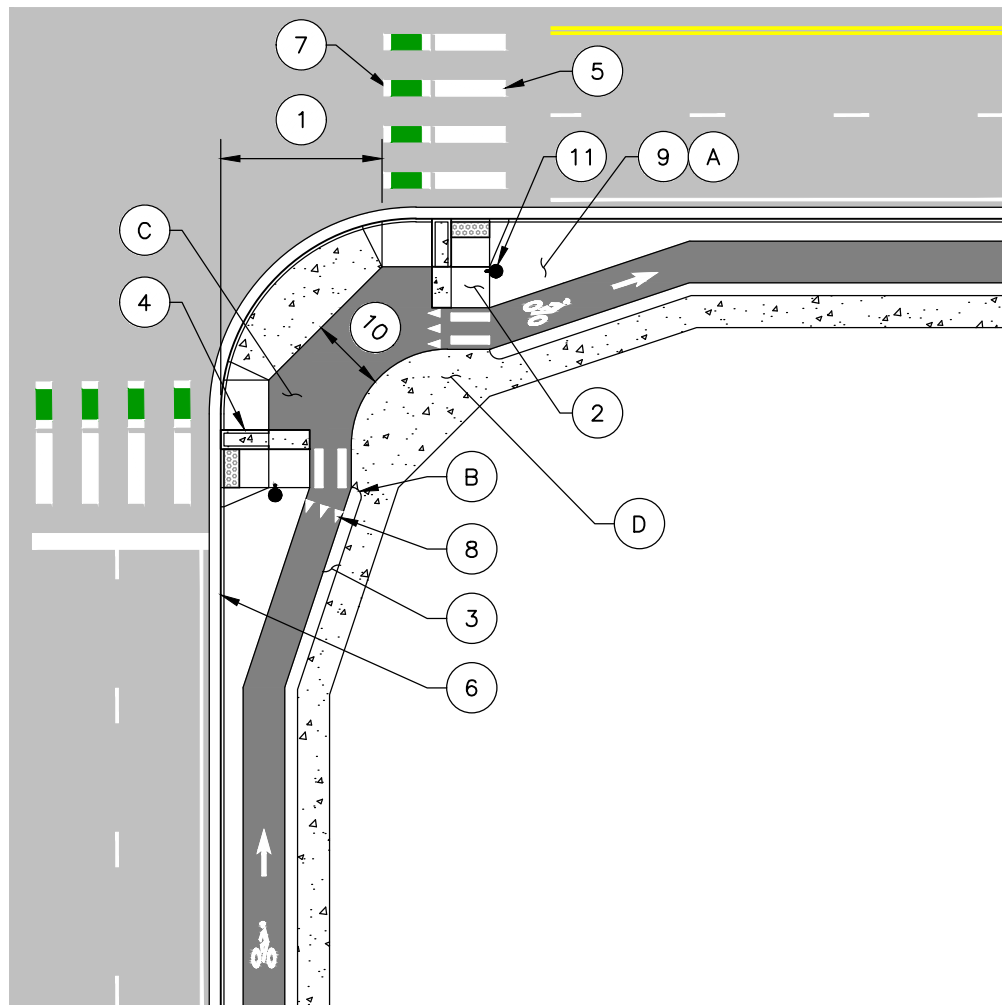
SHEET 2 OF 9

EXHIBIT NO.  
Item 7.  
RBL-02

RAISED BIKE LANE EXAMPLE APPLICATIONS  
PROTECTED INTERSECTION



# **ELEMENTS OF A PROTECTED INTERSECTION (SIGNALIZED) BUFFER SEPARATED RAISED BIKE LANE TO MIXED USE CORNER**



## **NOTES**

- 1 16' MINIMUM SETBACK FROM FACE OF CURB TO BIKE CROSSING TO IMPROVE VISIBILITY FOR VEHICLES, PEDESTRIANS, AND BICYCLISTS. INCREASED SETBACK IS DESIRABLE TO PROVIDE SPACE FOR A YIELDING VEHICLE WITHOUT BLOCKING THROUGH TRAFFIC.
- 2 ADA COMPLIANT PEDESTRIAN LANDING AREA, 4FT BY 4' MINIMUM.
- 3 2' MINIMUM WIDTH FOR SIDEWALK BUFFER. BUFFER SHALL BE ADA DETECTABLE.
- 4 COMBINATION PERPENDICULAR CURB RAMP WITH 3' VERTICAL CURB SEPARATOR.
- 5 CROSSWALK MARKINGS PER ACHD STANDARD DRAWING TS-1112.03.
- 6 6" VERTICAL CURB & GUTTER PER ACHD STANDARD DRAWING SD-701.
- 7 BIKE LADDER MARKINGS PER DETAIL A, SHEET 2.
- 8 YIELD MARKINGS PER ACHD STANDARD DRAWING TS-1113.06.
- 9 AREA MAY BE UTILIZED FOR SIGNAL EQUIPMENT AND OTHER TRAFFIC CONTROL DEVICES.
- 10 10 FT MINIMUM TO PROVIDE QUEUE STORAGE AND BIKE MIXING ZONE MANEUVERABILITY.
- 11 PEDESTRIAN PUSH BUTTON POLE EQUIPPED WITH APS PER ACHD STANDARD DRAWING TS-1106.02.
- A BIKE DETECTION DEVICES INCLUDE PUSH BUTTONS, LOOP DETECTORS, CAMERAS, OR RADAR. DETECTION PREFERENCE TO BE DETERMINED BY ACHD DURING PROJECT DEVELOPMENT.
- B MIXED USE ZONE BEGINS AT THE END OF SIDEWALK BUFFER. PUSH BUTTON LOCATOR TONES TO BE UTILIZED WITH THE APS BUTTONS FOR ADA CHANNELIZATION.
- C BIKE LANE AREA MAY BE PAINTED GREEN TO INDICATE AND REINFORCE BIKE CHANNELIZATION.
- C MIXED USE CORNER TREATMENT TO BE USED WHEN INTERSECTING MULTI-USE PATHWAY.

SHEET 3 OF 9

EXHIBIT NO.  
Item 7.  
RBL-03

RAISED BIKE LANE EXAMPLE APPLICATIONS  
PROTECTED INTERSECTION



# ELEMENTS OF A ROUNDABOUT CURB SEPARATED RAISED BIKE LANES

CURB SEPARATED  
BIKE LANE SHOWN (A)

EXAMPLE CORNER WITH  
PARALLEL RAMP

EXAMPLE CORNER WITH PERPENDICULAR  
RAMPS AND PEDESTRIAN LANDING AREA

## NOTES

- 1 THE BICYCLE CROSSING AND PEDESTRIAN CROSSING SHALL BE AT THE SAME ELEVATION AND PARALLEL TO ONE ANOTHER.
- 2 COMBINATION PERPENDICULAR CURB RAMP WITH VERTICAL CURB SEPARATOR.
- 3 4' BY 4' PEDESTRIAN LANDING FOR PUSH BUTTON. LANDING AREA NOT NECESSARY AT LOCATIONS WHERE RRFBs AREN'T WARRANTED.
- 4 2" VERTICAL CURB. REFER TO DETAIL A, SHEET 1.
- 5 PARALLEL RAMP WITH 2" DROP TO BIKE LANE ELEVATION.
- 6 PARALLEL RAMP WITH 8' WINGS FOR DROP TO ROADWAY GRADE.
- A THE SAME PRINCIPALS APPLY TO BOTH CURB SEPARATED AND BUFFER SEPARATED BIKE LANES, INTENT IS TO BRING SIDEWALK AND BIKE LANE TO SAME ELEVATION FOR THE CROSSING

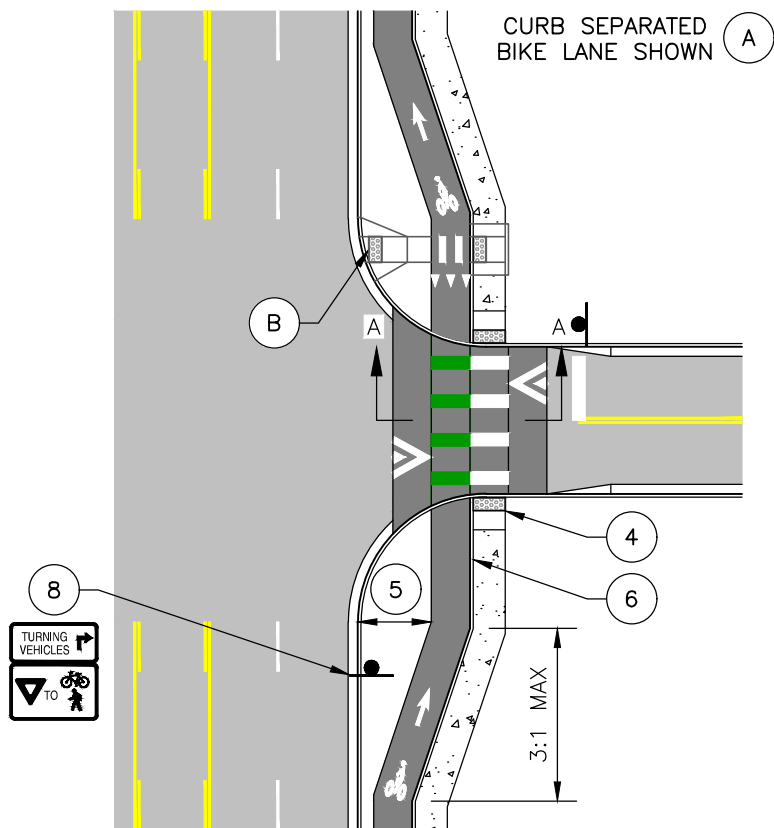
SHEET 4 OF 9

EXHIBIT NO.  
Item 7.  
RBL-04

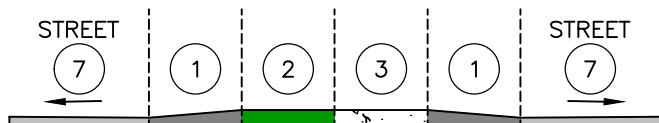
RAISED BIKE LANE EXAMPLE APPLICATIONS  
ROUNDABOUT



## RAISED SIDE STREET CROSSING



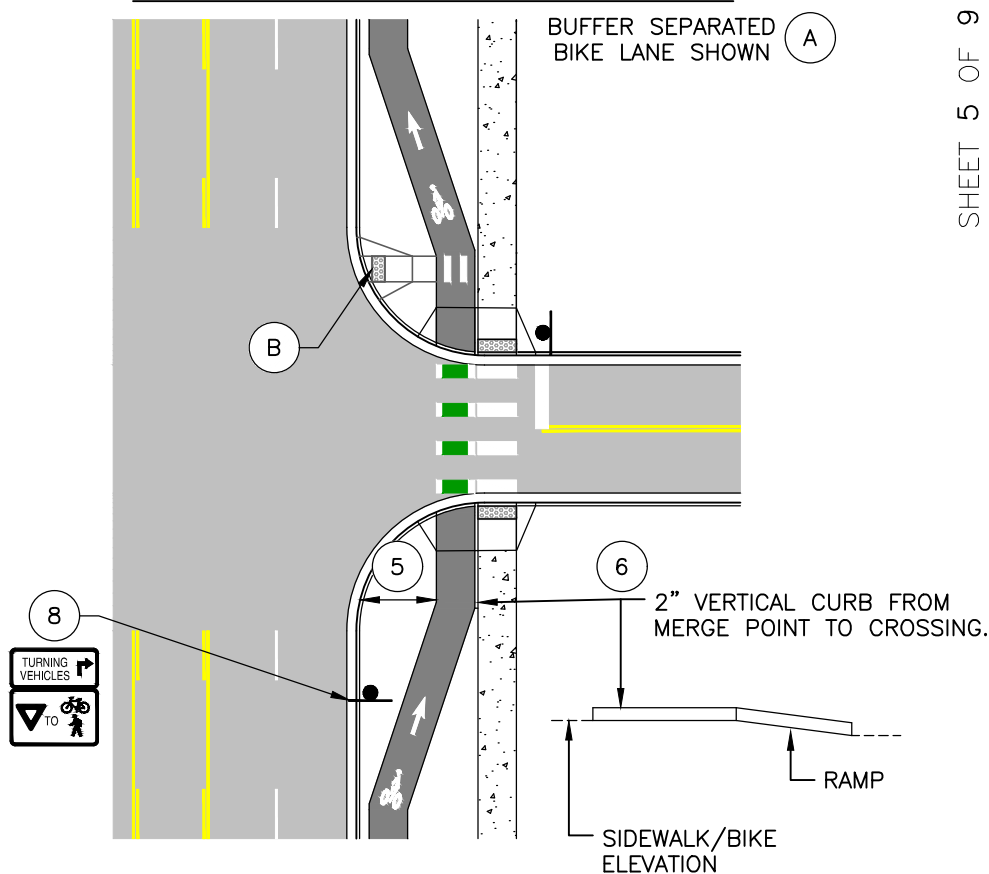
**DETAIL A**



## NOTES

- 1 APPROACH RAMP AND DEPARTURE RAMP SHALL NOT EXCEED 12:1 SLOPE. SPEED HUMP MARKINGS SHALL BE USED ON TRANSITION RAMPS.
- 2 6.5' DESIRED WIDTH AT SIDE STREET CROSSINGS, 5' MINIMUM.
- 3 5' MINIMUM WIDTH AT SIDE STREET CROSSINGS. THE CROSSING SHALL MEET ADA REQUIREMENTS.
- 4 PARALLEL RAMP WITH 2" DROP TO BIKE LANE & CROSSING LEVEL.

## AT-GRADE SIDE STREET CROSSING



- 5 6' TO 16' FOR 35 TO 45 MPH MAINLINE. 16' TO 24' FOR 45 TO 55 MPH MAINLINE. MINIMUM SETBACK FROM FACE OF CURB TO BIKE CROSSING TO IMPROVE VISIBILITY FOR VEHICLES, PEDESTRIANS, AND BICYCLISTS.
- 6 2" VERTICAL CURB. REFER TO DETAIL A, SHEET 1.
- 7 STORMWATER FACILITIES SHALL BE CONSIDERED ON A CASE BY CASE BASIS. RAISED CROSSING IS IDEAL WHEN SIDE STREETS/APPROACHES SLOPE AWAY FROM THE ROADWAY.
- A THE SAME PRINCIPALS APPLY TO BOTH CURB SEPARATED AND BUFFER SEPARATED BIKE LANES, INTENT IS TO BRING SIDEWALK AND BIKE LANE TO SAME ELEVATION FOR THE CROSSING
- B CROSSING OF THE MAIN STREET TO BE PROVIDED IF SIDE STREET IS A PUBLIC ROADWAY. MAY NOT BE REQUIRED FOR COMMERCIAL DRIVEWAYS. SEE INTERSECTION CROSSING DETAILS.

SHEET 5 OF 9

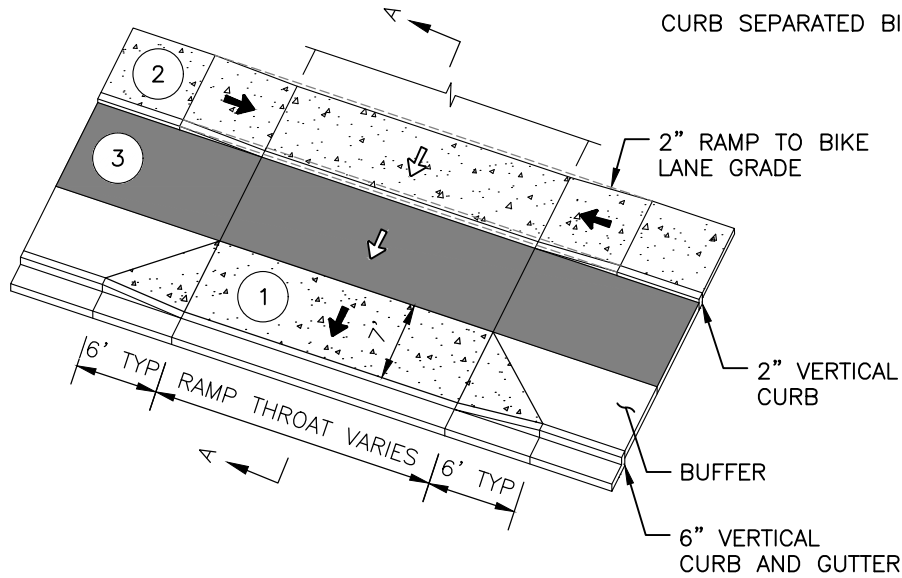
EXHIBIT NO.  
RBL-05

RAISED BIKE LANE EXAMPLE APPLICATIONS  
STOP CONTROLLED SIDE STREETS & APPROACHES



## SETBACK CONCRETE DRIVEWAY APPROACH RAISED BIKE LANE AND SIDEWALK

CURB SEPARATED BIKE LANE SHOWN (A)

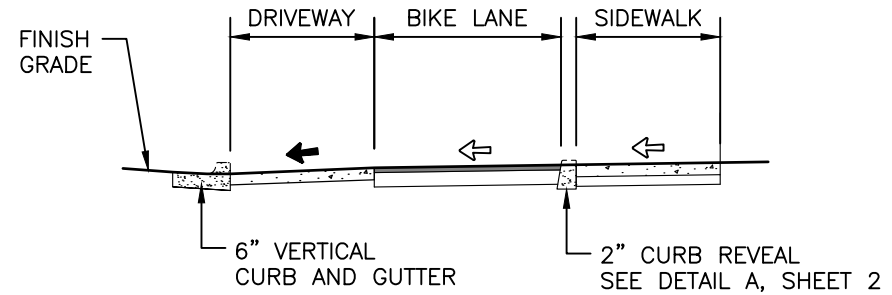


### LEGEND

- ↔ 1.5% ± 0.5% (2.0% MAX) SLOPE
- ← 7.3% ± 1.0% (8.3% MAX) SLOPE

### DETAIL A

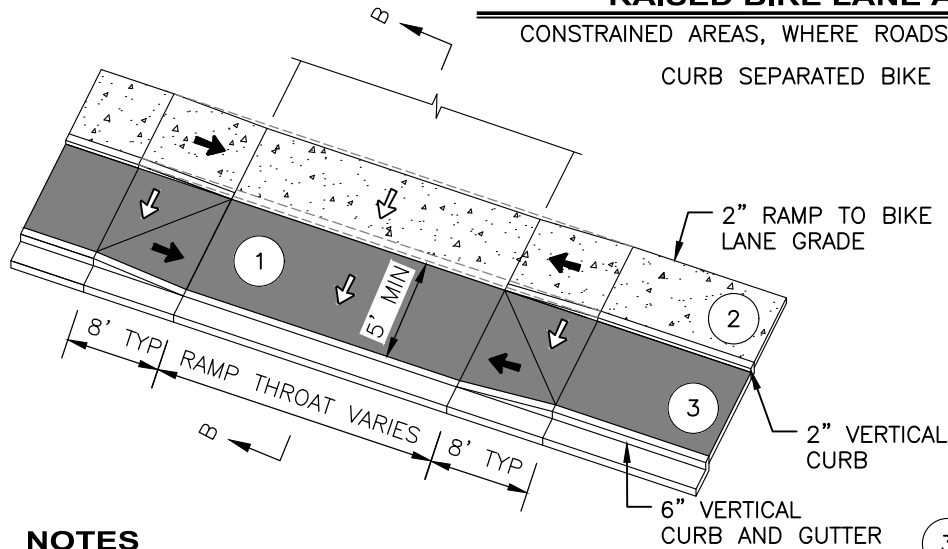
NOT TO SCALE



## ZERO SETBACK CONCRETE DRIVEWAY APPROACH RAISED BIKE LANE AND SIDEWALK

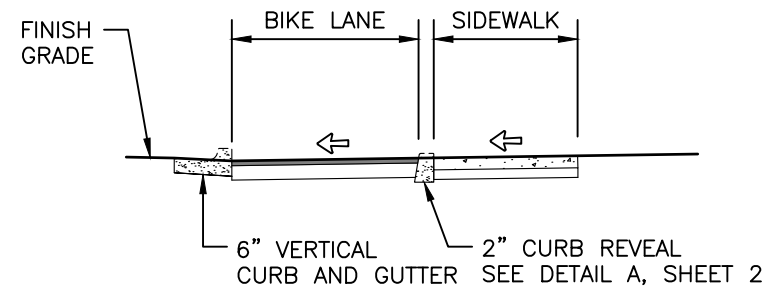
CONSTRAINED AREAS, WHERE ROADSIDE BUFFER IS IMPRACTICAL

CURB SEPARATED BIKE LANE SHOWN (A)



### DETAIL B

NOT TO SCALE



### NOTES

- 1 DRIVEWAY APPROACH DIMENSIONS ARE BASED ON 6" VERTICAL CURB AND GUTTER. FOR ADDITIONAL INFORMATION NOT SHOWN ON THIS SHEET, REFER TO ACHD STANDARD DRAWING SD-710.
- 2 SIDEWALK MINIMUM WIDTH IS 5' AT THE DRIVEWAY APPROACH.
- 3 5' MINIMUM WIDTH AT DRIVEWAYS, AT CONSTRAINED AREAS, AND FOR ONE-WAY BIKE LANES. 6.5' DESIRED MINIMUM WIDTH TO ALLOW FOR SIDE-BY-SIDE RIDING OR PASSING. THE SURFACE MATERIAL OF THE RAISED BIKE LANE SHALL CONTINUE THROUGH THE DRIVEWAY.
- A THE SAME PRINCIPALS APPLY TO BOTH CURB SEPARATED AND BUFFER SEPARATED BIKE LANES, INTENT IS TO BRING SIDEWALK AND BIKE LANE TO SAME ELEVATION FOR THE CROSSING

SHEET 6 OF 9

Item 7.

EXHIBIT NO.

RBL-06

RAISED BIKE LANE EXAMPLE APPLICATIONS

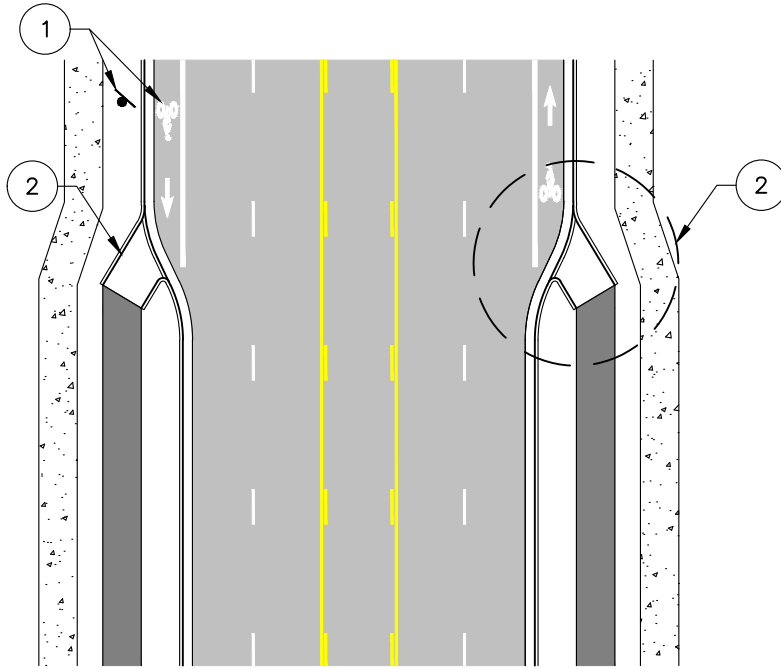
DRIVEWAY APPROACHES



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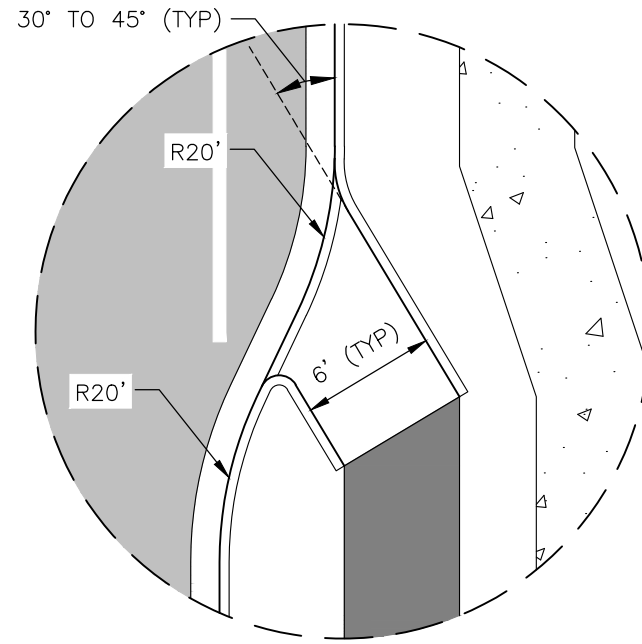
# **RAISED BIKE LANE TRANSITION TO CONVENTIONAL BIKE LANE**



## **NOTES**

- BIKE LANE MARKING PER ACHD STANDARD DRAWING TS-1113.04. MARKING SHALL BE PLACED IMMEDIATELY BEFORE AND AFTER A BIKE TRANSITION RAMP AND THEN APPROXIMATELY EVERY 500 FT. R7-9 SIGN MAY BE REQUIRED AT THE DISCRETION OF THE ENGINEER.
- BIKE TRANSITION RAMP SHALL BE PLACED WITHIN THE BUFFER SPACE, WHERE POSSIBLE. THE DESIRED SLOPE OF THE RAMP IS 12:1, BUT MAY BE STEEPER AT CONSTRAINED LOCATIONS. REFER TO DETAIL A, THIS SHEET FOR ADDITIONAL INFORMATION.

## **DETAIL A** NOT TO SCALE



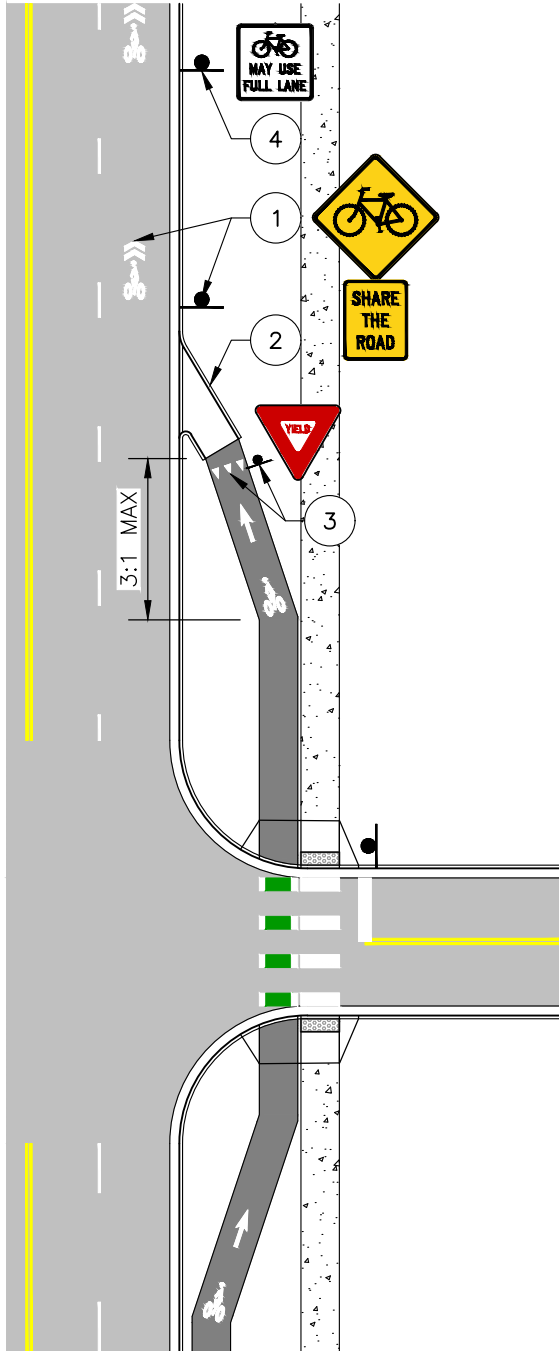
SHEET 7 OF 9

EXHIBIT NO.  
RBL-07

RAISED BIKE LANE EXAMPLE APPLICATIONS  
TRANSITION TO CONVENTIONAL BIKE LANE



# RAISED BIKE LANE TRANSITION TO SHARED LANE



## NOTES

- 1 SHARED LANE MARKING PER ACHD STANDARD DRAWING TS-1113.05. SIGNAGE (W11-1 AND W16-1P MOUNTED BELOW) SHALL BE PROVIDED NEAR BIKE RAMP TO ALERT VEHICLES OF BICYCLISTS ENTERING THE ROADWAY.
- 2 BIKE TRANSITION RAMP SHALL BE PLACED WITHIN THE BUFFER SPACE, WHERE POSSIBLE. THE DESIRED SLOPE OF THE RAMP IS 12:1, BUT MAY BE STEEPER AT CONSTRAINED LOCATIONS. REFER TO DETAIL A, SHEET 7 FOR ADDITIONAL INFORMATION.
- 3 YIELD MARKINGS AND SIGNAGE (R1-2) SHALL BE PROVIDED NEAR TOP OF RAMP PER ACHD STANDARD DRAWING TS-1113.06.
- 4 R4-11 SIGN MAY BE INSTALLED ALONG A NARROW ROAD WHERE BICYCLISTS AND VEHICLES CANNOT OPERATE SIDE BY SIDE.

SHEET 8 OF 9

Item 7.

EXHIBIT NO.

RBL-08

RAISED BIKE LANE EXAMPLE APPLICATIONS  
TRANSITION TO SHARED LANE



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3775 Adams Street, Boise, Idaho, 83714  
www.achd.idaho.gov

## REFERENCES FOR RAISED BIKE LANES

1. ADA COUNTY HIGHWAY DISTRICT. "2017 ACHD SUPPLEMENT TO THE 2017 ISPC." DECEMBER 2017, [HTTPS://WWW.ACHDIDAHO.ORG/DOCUMENTS/ENGINEERING/ISPC/2017\\_ISPCSUPPLEMENTS.PDF](https://www.achdidaho.org/documents/engineering/ispc/2017_ispcsupplements.pdf).
2. CITY OF WEST LINN PUBLIC WORKS DEPARTMENT. SEPARATED BIKE PATH AT INTERSECTION STANDARD DRAWINGS. REVISED FEBRUARY 2019, [HTTPS://WESTLINNOREGON.GOV/PUBLICWORKS/STANDARD-DRAWINGS](https://westlinnoregon.gov/publicworks/standard-drawings).
3. MASSACHUSETTS DEPARTMENT OF TRANSPORTATION. "SEPARATED BIKE LANE PLANNING & DESIGN GUIDE." 2015, [HTTPS://WWW.MASS.GOV/LISTS/SEPARATED-BIKE-LANE-PLANNING-DESIGN-GUIDE](https://www.mass.gov/lists/separated-bike-lane-planning-design-guide).
4. NATIONAL ASSOCIATION OF CITY TRANSPORTATION OFFICIALS. "URBAN BIKEWAY DESIGN GUIDE ANNOTATED PLANS." APRIL 2011.
5. U.S. DEPARTMENT OF TRANSPORTATION FEDERAL HIGHWAY ADMINISTRATION. "SEPARATED BIKE LANE PLANNING AND DESIGN GUIDE." MAY 2015.

SHEET 9 OF 9

Item 7.

EXHIBIT NO.

RBL-09

RAISED BIKE LANE EXAMPLE APPLICATIONS  
REFERENCES



35

## Call for Comments on TIP Amendment Regarding I-84

# Now open!

## Comment on proposed transportation projects now

**Public comment period:**  
**October 26 - November 9, 2021**

Comments are being accepted on proposed amendments to *Communities in Motion 2040 2.0* (CIM 2040 2.0), the region's long-range transportation plan, and the FY2021-2027 and FY2022-2028 Regional Transportation Improvement Programs (TIPs).

### **The proposed amendments would add the following project to both CIM 2040 2.0 and the TIPs:**

- I-84, Meridian Road to Eagle Road in the City of Meridian
  - Evaluate adding an auxiliary lane, including an additional lane and shoulder on the eastbound on-ramp and eastbound off-ramp at Meridian Road. Work includes all studies and design work necessary. Traffic patterns from Meridian Road to the WYE interchange will also be studied. (Construction currently unfunded)

### **The proposed amendments would also remove the following projects from the TIPs:**

- Two bicycle parking facilities at Boise State University
- A reconfiguration project on Holly Street near Northwest Nazarene University in the City of Nampa
- A rehabilitation project sponsored by the Ada County Highway District

Comments must be submitted in writing (via email or letter) and received no later than **11:59 pm, Tuesday, November 9, 2021.**



## How do I submit my comments?

Please direct comments or questions to Toni Tisdale, Principal Planner:

- **Email:** [ttisdale@compassidaho.org](mailto:ttisdale@compassidaho.org)
- **Mail:** COMPASS, 700 NE 2<sup>nd</sup> Street, Suite 200, Meridian, ID 83642
- **Fax:** 208/855-2559

**Comments due by 11:59pm, Tuesday, November 9, 2021.**

Were you forwarded this email and want to receive updates from us in the future? Join our mailing list by emailing: [info@compassidaho.org](mailto:info@compassidaho.org)

Those needing assistance, including assistance in submitting written comments, may call 208/855-2558 with 48 hours advance notice.

*Personas que necesitan asistencia especial, favor de llamar al número 208/855-2558 con 48 horas de anticipación.*

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### COMPASS Public Participation Workgroup

*Now recruiting a new member for the*

## COMPASS Public Participation Workgroup

**Do you care about the future of the Treasure Valley?**

**Is it important to you that everyone who lives here has a say in that future?**

**Join the COMPASS Public Participation Workgroup  
to help make that happen.**

COMPASS is recruiting a member of the public for its Public Participation Workgroup – a group of individuals who provide input and assistance into COMPASS' communication and public involvement programs to help ensure everyone who lives in Ada and Canyon Counties has an opportunity to be involved in planning for the future.

COMPASS is currently seeking to fill one seat on the workgroup with a member of the public. Applications are accepted at any time; once the seat is filled, additional applications can remain on file for future openings.

Interested? Click below to learn more, review the workgroup charter, and find the membership application.

[Learn More and Apply](#)

Questions?

Contact Amy Luft at 208-475-2229 or [aluft@compassidaho.org](mailto:aluft@compassidaho.org).

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**Keeping Up With COMPASS**

# Keeping Up With COMPASS

October 2021

**A newsletter for COMPASS members to keep abreast of COMPASS Board, committee, and workgroup actions.**

## **Executive Committee – September 14, 2021**

More information: [www.compassidaho.org/people/execmeetings.htm](http://www.compassidaho.org/people/execmeetings.htm)

Next meeting date: October 12, 2021

### **September Action Items:**

- October Board Meeting Agenda. Established the agenda for the October 18, 2021, COMPASS Board of Directors meeting.
- Board Officer Nominations. Authorized Executive Director Matt Stoll to solicit nominations for the 2022 Secretary/Treasurer position on the COMPASS Board of Directors.
- FY2021-2027 TIP. Approved a Board administrative modification to increase the cost of a rail crossing project at Look Lane, west of the City of Caldwell, at the request of the Idaho Transportation Department. The COMPASS Board of Directors will be requested to ratify the Executive Committee's action in its October 18, 2021, meeting.
- NARC Board of Directors. Recommended COMPASS Board of Directors' approval to appoint Ada County Highway District Commissioner Mary May to fulfill the remainder of Mayor Garret Nancolas' position representing COMPASS on the National Association of Regional Councils (NARC) Board of Directors. This item will be brought to the COMPASS Board of Directors for action in its October 18, 2021, meeting.

### **September Information/Discussion Items:**

- 2022 State and Federal Legislative Positions. Discussed potential areas of focus for the 2022 state and federal legislative sessions. Draft position statements will be presented to the Executive Committee at its November 9, 2021, meeting.

## **Regional Transportation Advisory Committee (RTAC) – September 22, 2021**

More information: [www.compassidaho.org/people/rtacmeetings.htm](http://www.compassidaho.org/people/rtacmeetings.htm)

Next meeting date: November 17, 2021

**NOTE: The October 27, 2021, RTAC meeting has been cancelled.**

### **September Action Items:**

- Resource Development Plan. Recommended COMPASS Board of Directors' approval of the FY2022 Resource Development Plan. This item will be brought to the COMPASS Board of Directors for action in its October 18, 2021, meeting.
- CIM 2050 Performance Measures. Recommended COMPASS Board of Directors' approval of *Communities in Motion 2050* (CIM 2050) performance measures and targets. This item will be brought to the COMPASS Board of Directors for action in its October 18, 2021, meeting.

- CIM 2040 2.0 and TIP. Recommended COMPASS Board of Directors' approval of amendments to *Communities in Motion 2040 2.0* (CIM 2040 2.0) and the FY2021-2027 Regional Transportation Improvement Program (TIP) and approval of the FY2022-2028 TIP and associated air quality conformity demonstration. This item will be brought to the COMPASS Board of Directors for action in its October 18, 2021, meeting.
- CIM 2050 Funding Policy. Recommended COMPASS Board of Directors' approval of a funding policy for CIM 2050. This item will be brought to the COMPASS Board of Directors for action in its October 18, 2021, meeting.

#### September Information/Discussion Items:

- ACHD Livable Streets. Received an overview of Ada County Highway District's (ACHD's) livable streets performance measure and targets.
- Travel Survey. Received an update on the 2021 COMPASS household travel survey.
- CIM 2050 Transportation Needs. Received an overview of the proposed process for identifying roadway and public transportation needs for CIM 2050, to feed in to the CIM 2050 project prioritization process. Staff will present a draft list of potential needs in the November 17, 2021, RTAC meeting and request approval to begin technical analyses.

## Workgroups

### Active Transportation Workgroup

Meeting date: September 13, 2021

Highlights:

- Received a progress report on the Rail with Trail project.
- Discussed the status of the regional pathways plan's prioritization process.

*Next meeting date:* October 2021

### Environmental Review Workgroup

Meeting date: September 23, 2021

Highlights:

- Reviewed regional rail on the Boise Cutoff alignment as the approved "locally favored" high-capacity transit option and discussed the public survey feedback about high-capacity transit needs and preferences.
- Received a Planning and Environmental Linkages process overview, discussed a local example, and discussed how to apply the process to advance planning for regional rail.

*Next meeting date:* November 2021

**Access past editions of *Keeping Up with COMPASS* online at**  
[www.compassidaho.org/comm/newsletters.htm](http://www.compassidaho.org/comm/newsletters.htm).





# MEMO TO TRANSPORTATION COMMISSION

## Agenda Topic on the Transportation Commission Agenda

**From:** Miranda Carson, Coordination Planner      **Meeting Date:** November 1, 2021  
**Presenter:** Walter Steed, Chair      **Estimated Time:** 5-10 minutes  
**Topic:** ACHD Capital Investment Citizens Advisory Committee (CICAC) Volunteer

Created in 1991 to serve as a recommending body for the ACHD Commission, the Capital Investments Citizens Advisory Committee (CICAC) is composed of citizens and planners from each of the cities and Ada County. Members assist the District by reviewing the capital improvement portion of the annual budget, mid-year budget adjustments for capital improvements, the Integrated Five-Year Work Plan, the Capital Improvement Plan (CIP), as well as other major programs/projects. The CICAC plays a specific role in the Impact Fee Program, which the group has a role defined by Idaho law.

Per recently passed House Bill 124 amending Idaho Code Section 67-8205 2.c: Employees or officials acting in their official capacity for a governmental entity may not be appointed as members of the committee.

(c) New appointments and reappointments to a committee on and after July 1, 2021, must comply with the provisions of this paragraph. All members must reside within the jurisdictional boundaries of the governmental entity. Two (2) or more members shall be active in the business of development, building, or real estate. Two (2) or more members shall not be in the business of development, building, or real estate. Employees or officials acting in their official capacity for a governmental entity may not be appointed as members of the committee. An existing planning or planning and zoning commission may serve as the development impact fee advisory committee for the governing authority if the commission includes two (2) or more members who are active in the business of development, building, or real estate and two (2) or more members who are not in such business; otherwise, two (2) such members who are not employees or officials of a governmental entity shall be appointed to the committee until the membership requirements of this subsection are met.

City staff has had a seat on CICAC that will now be relinquished. The Mayor's Office suggested a Transportation Commissioner fill the seat. The CICAC meets about quarterly. Typically meetings are held at ACHD, but recently those have been virtual. If interested please let me know before the end of the year.

For more information speak with Mr. Steed (CICAC member), Miranda Carson, or visit the committee webpage here: <https://www.achdidaho.org/Departments/Committees/CICAC.aspx>.